

# COPY

## ORDINANCE NO. 2006-03-026

AN ORDINANCE AMENDING PD ORDINANCE NO. 04-06-068 OF THE CITY OF MCKINNEY, TEXAS; SO THAT A 713 ACRE TRACT LOCATED ON THE NORTH SIDE OF U.S. HIGHWAY 380, SOUTH OF BLOOMDALE ROAD AND EAST OF STONEBRIDGE DRIVE IS REZONED FROM "PD" – PLANNED DEVELOPMENT DISTRICT TO "PD" – PLANNED DEVELOPMENT DISTRICT, TO AMEND THE DEVELOPMENT REGULATIONS FOR SINGLE FAMILY AND MIXED USES; PROVIDING REGULATIONS; PROVIDING FOR SEVERABILITY; PROVIDING FOR INJUNCTIVE RELIEF, PROVIDING FOR NO VESTED INTEREST; PROVIDING FOR THE PUBLICATION OF THE CAPTION OF THIS ORDINANCE; PROVIDING FOR A PENALTY FOR THE VIOLATION OF THIS ORDINANCE; AND PROVIDING FOR AN EFFECTIVE DATE HEREOF.

WHEREAS, the City of McKinney has considered the rezoning of an approximately 713 acre tract located on the north side of U.S. Highway 380, south of Bloomdale Road, and east of Stonebridge Drive, from "PD" – Planned Development District, to "PD" – Planned Development District, in order to amend the development regulations for single family and mixed uses and to modify the current general development plan, and,

WHEREAS, after due notice of the requested rezoning as required by law, and the required public hearings held before the Planning and Zoning Commission and the City Council of the City of McKinney, Texas, the City Council is of the opinion that the change in zoning district should be made.

**NOW THEREFORE BE IT HEREBY ORDAINED BY THE CITY COUNCIL OF THE CITY OF MCKINNEY, TEXAS:**

Section 1. Ordinance No. 04-06-068 is hereby amended so that an approximately 713 acre tract located on the north side of U.S. Highway 380, south of Bloomdale Road, and east of Stonebridge Drive, which is more fully depicted on Exhibit "A" attached hereto, is hereby rezoned to "PD" – Planned Development District, in order to amend the development regulations for single family and mixed uses and to modify the current general development plan.

Section 2. Use and development of the subject property shall conform to the regulations of Ordinance 1270, and as amended, except as follows:

- (a) The subject property shall generally develop in accordance with the attached Pattern Book regulations (Exhibit "B").
- (b) The number of single family residential units for the entire development may not exceed 2,177.
- (c) The Director of Planning has the authority to allow minor modifications to the Pattern Book as needed.
- (d) The applicant will be required to submit a Traffic Impact Analysis to the City Engineer for review

and approval prior to the approval of the preliminary-final plat for Phase 1.

- (e) If the proposed school site does not develop as shown in the Pattern Book, the area shall be designated as open space or public use facility.

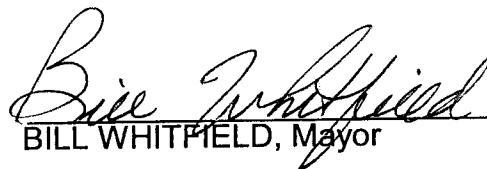
Section 3. If any section, subsection, paragraph, sentence, phrase or clause of this Ordinance shall be declared invalid for any reason whatsoever, such decision shall not affect the remaining portions of this Ordinance, which shall remain in full force and effect, and to this end, the provisions of this Ordinance are declared to be severable.

Section 5. It shall be unlawful for any person, firm or corporation to develop this property, or any portion thereof, in any manner other than is authorized by this Ordinance, and upon conviction therefore, shall be fined any sum not exceeding \$2,000.00, and each day that such violation shall continue shall be considered a separate offense. These penal provisions shall not prevent an action on behalf of the City of McKinney to enjoin any violation or threatened violation of the terms of this Ordinance, or an action for mandatory injunction to remove any previous violation hereof.


Section 6. That no developer or property owner shall acquire any vested interest in this Ordinance or specific regulations contained herein. The ordinance, and the subsequent site plans (if any) and regulations may be amended or repealed by the City Council of the City of McKinney, Texas, in the manner provided by law.

Section 7. The caption of this Ordinance shall be published one time in a newspaper having general circulation in the City of McKinney, and shall become effective upon such publication.

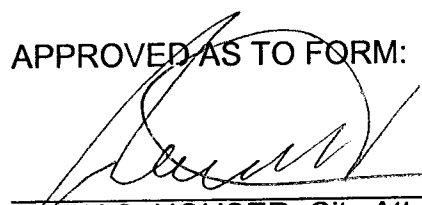
**DULY PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF MCKINNEY, TEXAS, ON THIS 7<sup>th</sup> DAY OF MARCH, 2006.**

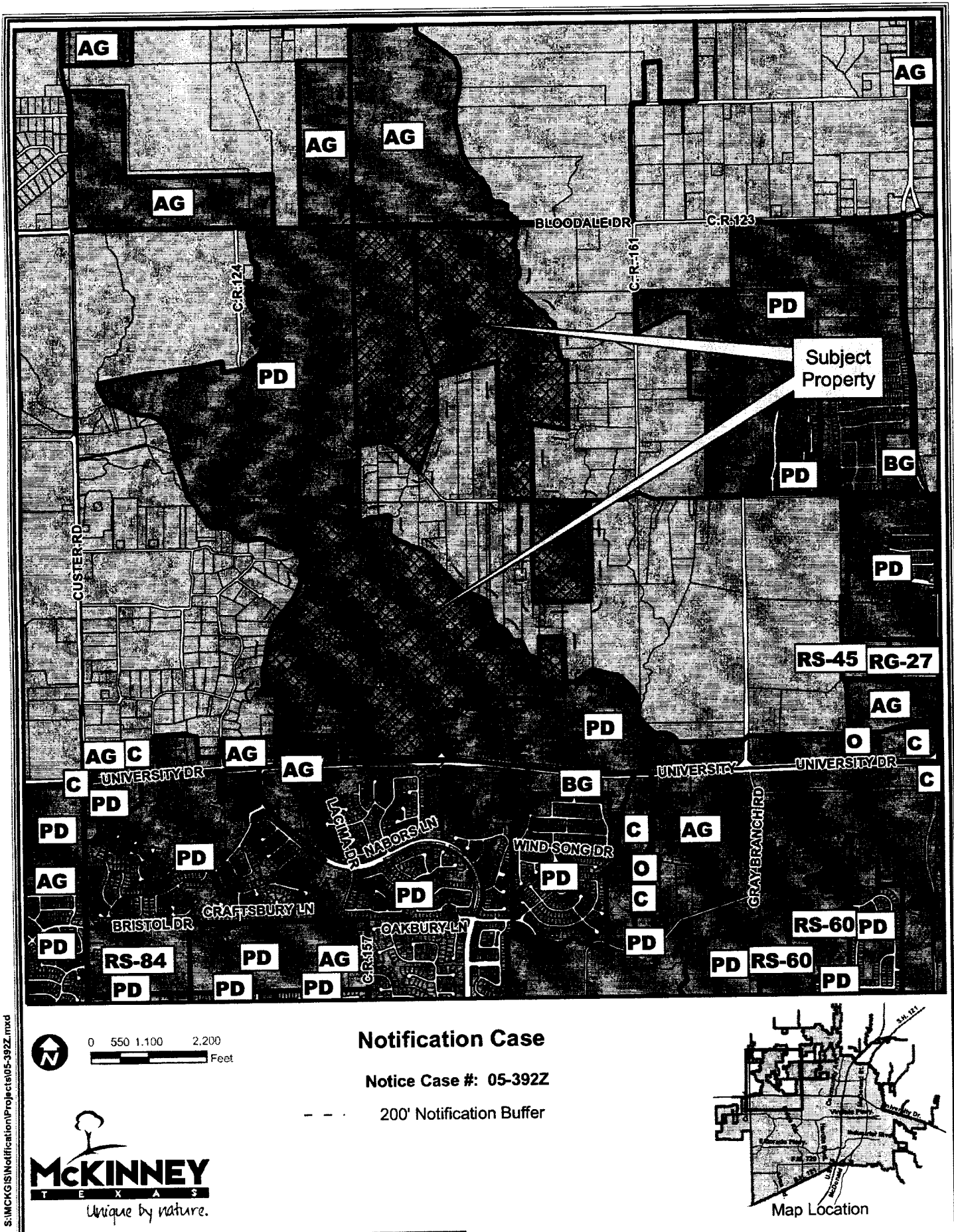
  
BILL WHITFIELD, Mayor

CORRECTLY ENROLLED:

  
SANDY HART, CMC  
City Secretary  
BEVERLY COVINGTON  
Deputy City Secretary

APPROVED AS TO FORM:

  
MARK S. HOUSER, City Attorney

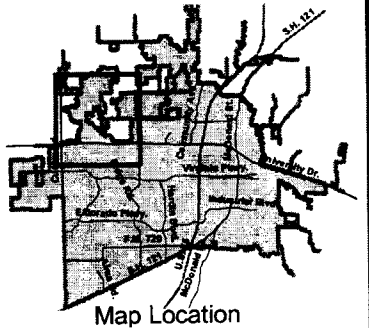
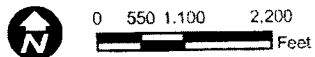


# Notification Case

Notice Case #: 05-3922

-- 200' Notification Buffer

S:\MCKGIS\Notification\Projects\05-3922.mxd



DISCLAIMER: This map and information contained in it were developed exclusively else is at that party's risk and without liability to the City of McKinney, it's officials or

## EXHIBIT "A"

S  
O  
U  
T  
H  
E  
R  
N  
  
L  
A  
N  
D



T  
R  
A  
C  
T

PATTERN BOOK  
ORDINANCE VERSION

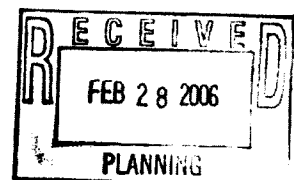


EXHIBIT "B"

## TABLE OF CONTENTS



### SECTION 1: PLAN

* Illustrative Regulating Plan	...4
* Land Use Plan	...5
* Building Type Density Chart	...6
* Bikeway Network Diagram	...7
* Public Open Space Diagram	...8
* Illustrative Details: North District	...9
* Illustrative Details: South District	...11

### SECTION 2: URBAN STANDARDS

* Transect System	...13
* Building Types by Transect	...14
* Permitted Uses by Transect	...15
* Building and Streetscape Standards	...16
* Building Type Summary: Introduction	...17
* Building Type Summary: Commercial	...18
* Building Type Summary: Residential	...19
* Lot Configuration Options	...21

### SECTION 3: THOROUGHFARE STANDARDS

* Thoroughfare Plan	...27
* Thoroughfare Summary	...29
* Thoroughfare Sections and Descriptions	...30
* Thoroughfare Diagrams	...37
* Thoroughfare Standards	...39

### SECTION 4: OPEN SPACE STANDARDS

...40

### SECTION 5: ARCHITECTURAL STANDARDS

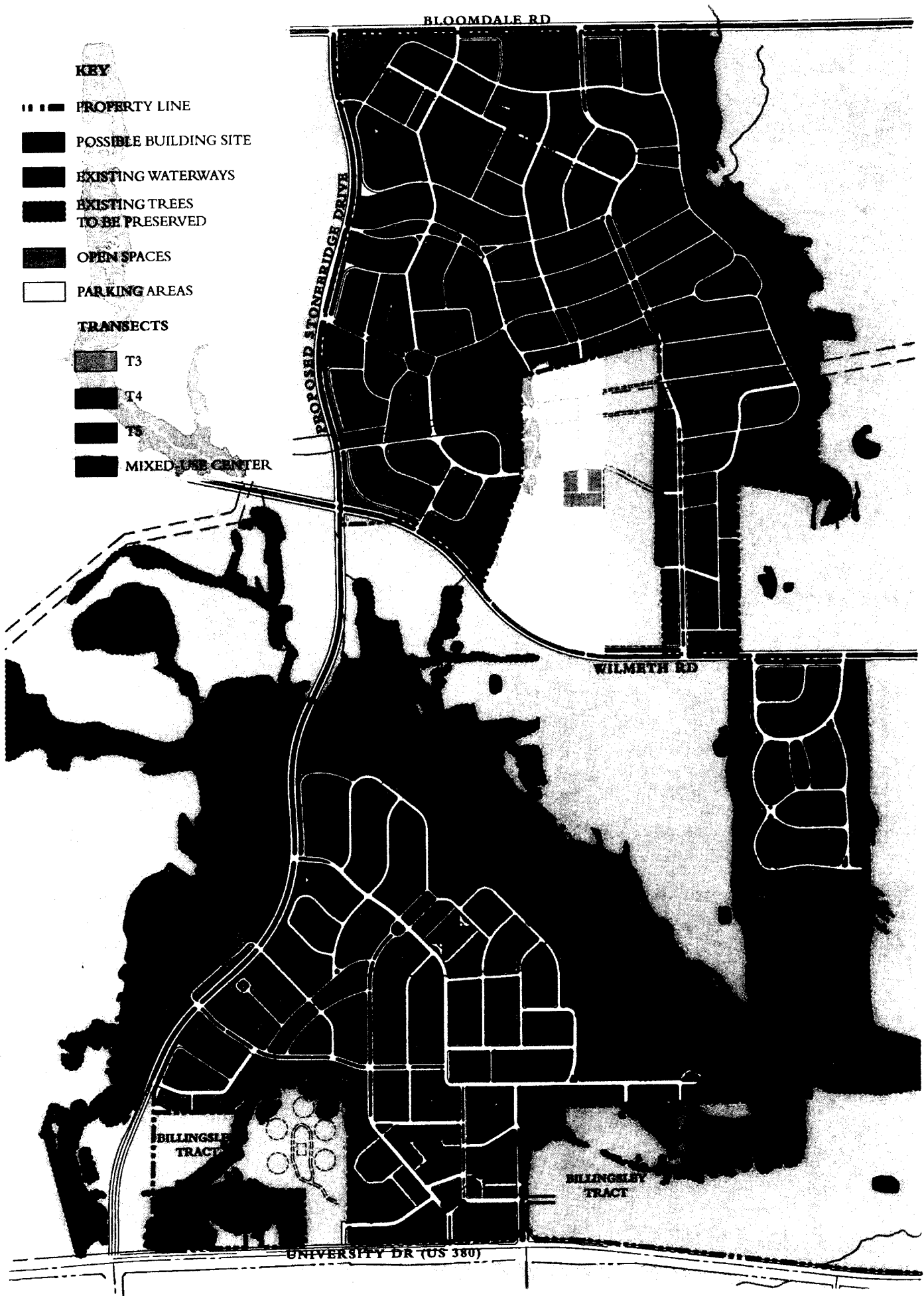
* Urban Center Architectural Code	...41
* Signage Standards	...44
* Fire Protection Requirements	...46

### APPENDIX

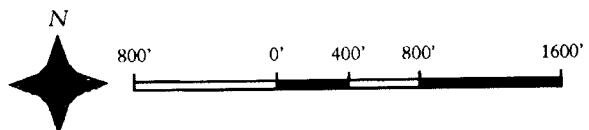
* Appendix A - Glossary of Terms	...47
----------------------------------	-------







- \* If designated School Site in South District is not developed as shown, that area shall be designated as Open Space or Public Use Facility.
- \*\* Any gated section of the Southern Land community is subject to the issuance of a Special Use Permit (SUP)





PLAN  
LAND USE PLAN

KEY

--- PROPERTY LINE

■ POSSIBLE BUILDING SITE

BUILDING TYPE	T3	T4	T5
VILLA			
NON-ALLEY			
ALLEY			
COTTAGE			
SHARED WALL			
TOWNHOUSE			
MIXED-USE			

NORTH DISTRICT  
TOTAL  
ACREAGE:  
323

WILMETH RD.  
PARCEL TOTAL  
ACREAGE:  
48

SOUTH DISTRICT  
TOTAL  
ACREAGE:  
342

800' 0' 400' 800' 1600'







	VILLA	NON-ALLEY	ALLEY	COTTAGE	SHARED WALL	TOWNHOME	LIVE/WORK & MIXED-USE	PLAN INDICATED TOTAL UNITS*
T3	100	273	111		30			514
T4		433	229	71	44			777
T5			101	150	30	97	310	688
TOTAL UNITS	100	706	441	221	104	97	310	1979 **

These values reflect product counts represented in the Illustrative Regulating Plan (at left) and General Development Plan as of February 2006. Values given for Live/Work and Mixed-Use Building Types refer to upper-level residential units only. Please refer to the Design Code portion of the book for specific information on Transect zoning and Building Type Summary.

Flexibility in Building Type Density is both permitted and expected. The following conditions shall apply:

- \* Within each of the three transects, each allowable Building Type shall comprise at least five (5%) percent of the total units within the transect.
- \* Within each of the three transects, no single Building Type shall not exceed seventy five (75%) percent of the total units within the transect.
- \*\* The total number of units in the project is flexible but shall not exceed 2177 residential units (110% of the given value).

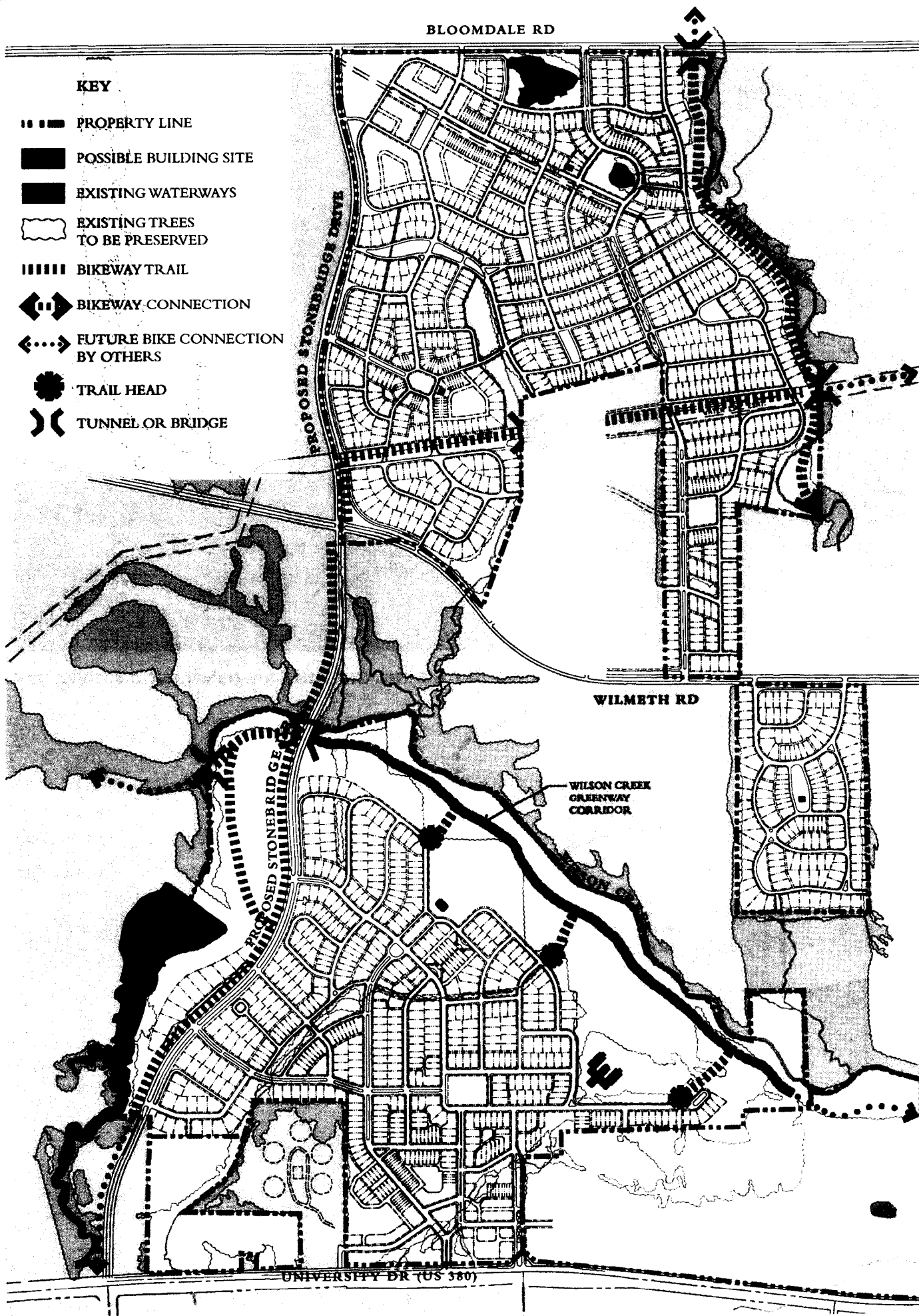




PLAN  
BIKEWAY NETWORK DIAGRAM

KEY

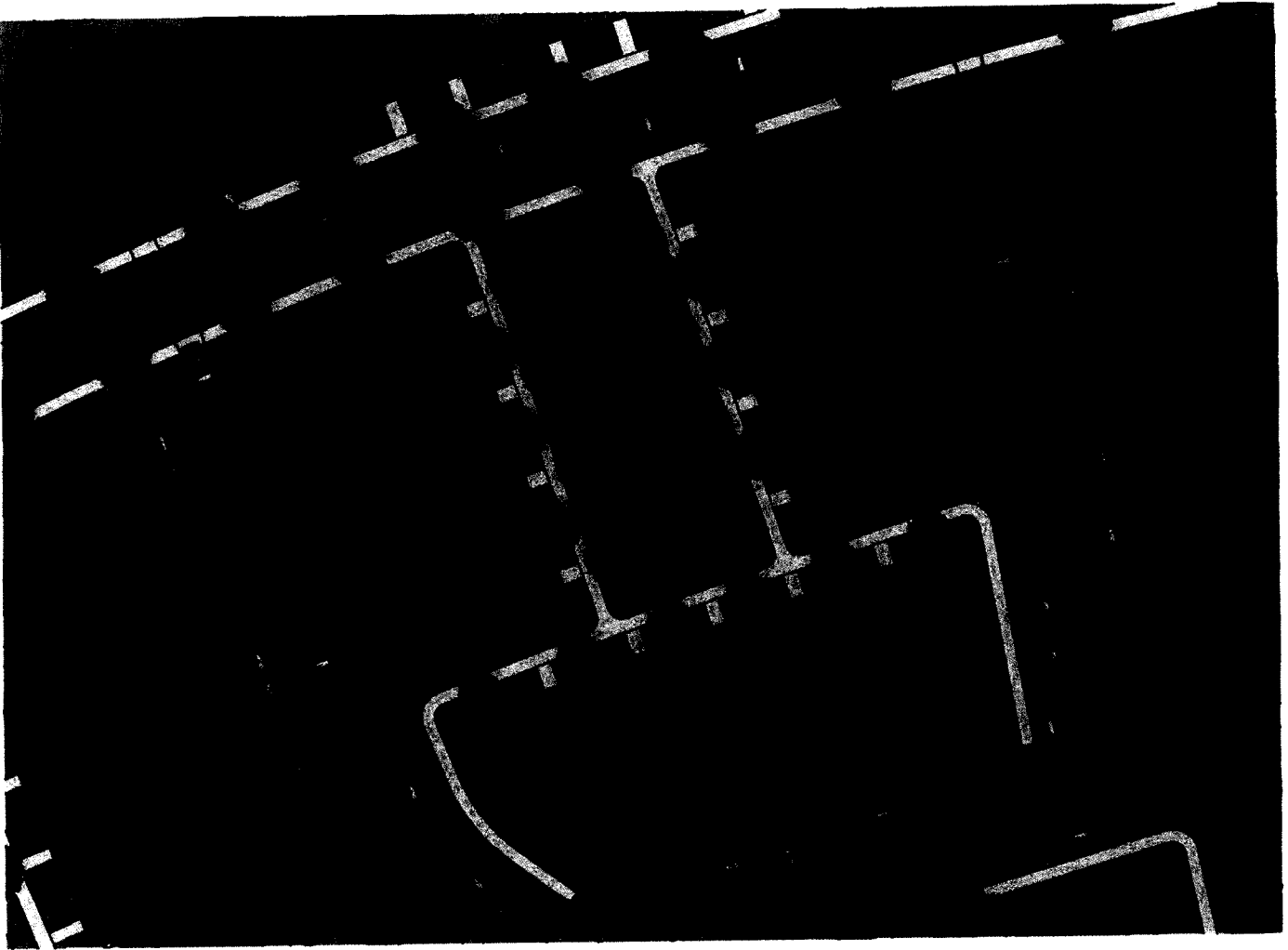
- PROPERTY LINE
- POSSIBLE BUILDING SITE
- EXISTING WATERWAYS
- ⬡ EXISTING TREES TO BE PRESERVED
- |||| BIKEWAY TRAIL
- ◀▶ BIKEWAY CONNECTION
- ◀...▶ FUTURE BIKE CONNECTION BY OTHERS
- ⊗ TRAIL HEAD
- )( TUNNEL OR BRIDGE



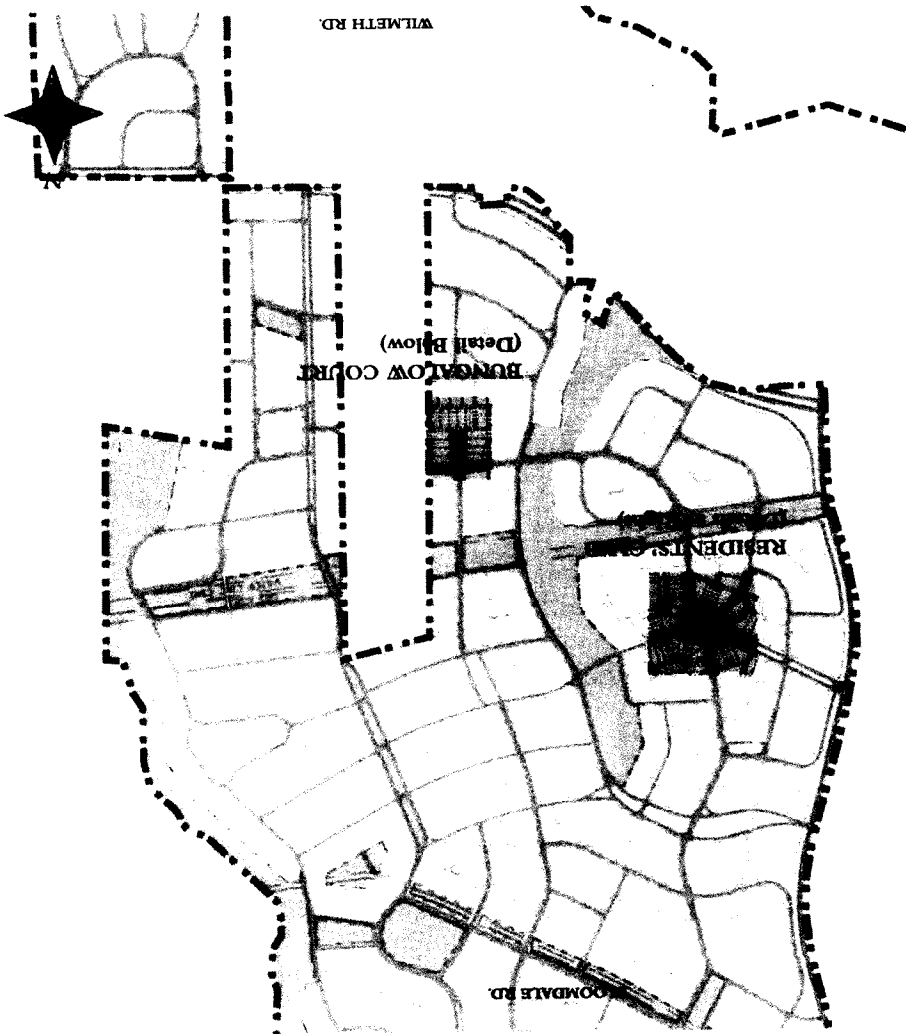
800' 0' 400' 800' 1600'



Rev. 02-23-06

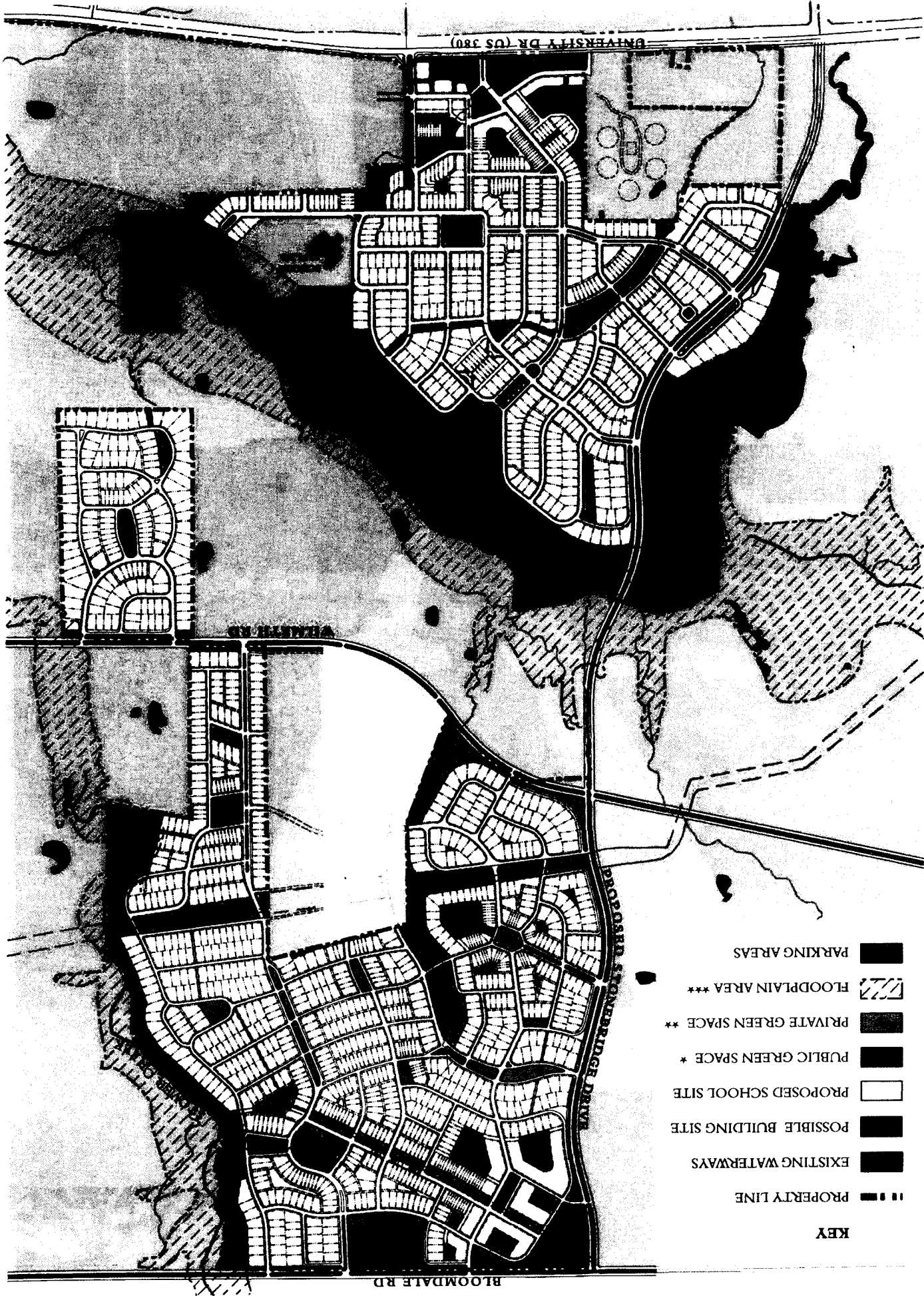


ALL IMAGES CONTAINED ON THIS AND THE FACING PAGE ARE FOR ILLUSTRATIVE PURPOSES ONLY.





# PLAN PUBLIC OPEN SPACE DIAGRAM



KEY

PROPERTY LINE

EXISTING WATERWAYS

POSSIBLE BUILDING SITE

PROPOSED SCHOOL SITE

PUBLIC GREEN SPACE \*

PRIVATE GREEN SPACE \*\*

FLOODPLAIN AREA \*\*\*

PARKING AREAS

\* Public Green Space to be maintained by City of McKinney  
\*\* Private Green Space to be maintained by Developer  
\*\*\* These areas are prone to being wet and not considered buildable area



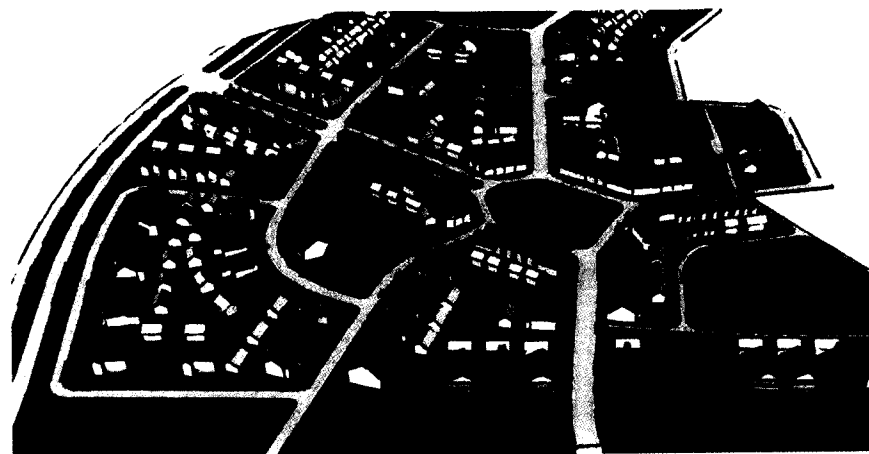
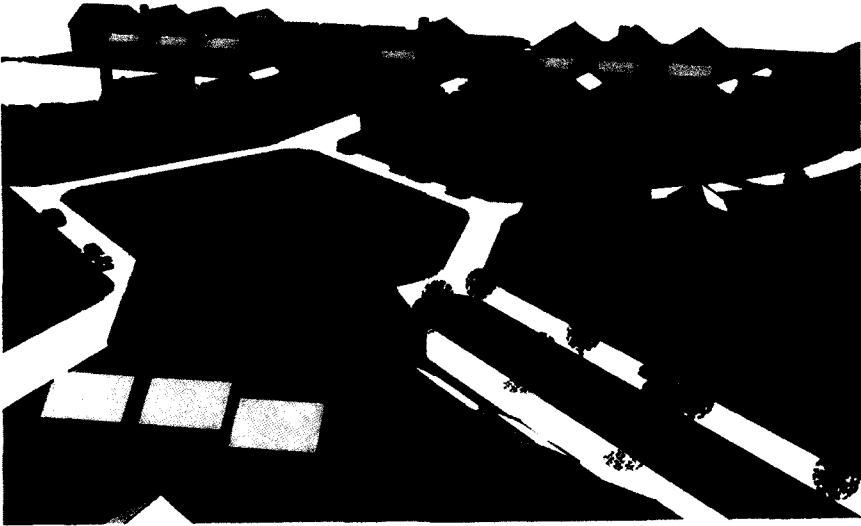
800' 400' 0' 400' 800' 1600'



Rev. 02-23-06

MCKINNEY, TEXAS

ILLUSTRATIVE DETAILS  
NORTH DISTRICT DETAILS





# ILLUSTRATIVE DETAILS

## SOUTH DISTRICT DETAILS



Rev. 02-23-06

SOUTHERN LAND TRACT PATTERN BOOK

ILLUSTRATIVE DETAILS  
SOUTH DISTRICT DETAILS





## DESIGN CODE

### TRANSECT SYSTEM

The **Design Code** is a document comprised of both text and graphics and intended to guide the creation of the Southern Land community. It is comprised of four sections: **Regulating Plan**, **Urban Standards**, **Thoroughfare Standards**, and **Architectural Standards**.

The Code also serves to assure that the following criteria are met in the Plan:

- \* The stated community is conceived as an arrangement of different urban Transect zones, each with specific requirements and provisions.
- \* Neighborhoods within the community are limited in size by a 5 to 7 minute (1/4 to 1/2 mile) walking distance from the edge to an easily identifiable corner.
- \* Residences, retail spaces, offices, and civic buildings are located in close proximity to each other to promote a true mixed-use environment.
- \* The streets, blocks, and placement of buildings on lots encourage pedestrian activity.
- \* Street networks are interconnected. Blocks are smaller and closer to the neighborhood centers.
- \* Recreation and basic neighborhood services are accessible by non-vehicular means from lots at the edge of neighborhoods.
- \* A variety of public spaces are provided for social gathering and recreation for all members of the community as well as outside visitors. Public spaces are defined by carefully articulated building frontages, and can include parks, greens, squares, plazas, and playgrounds.
- \* In addition to public spaces, civic buildings in prominent locations provide places for assembly and help to define the community as a whole.
- \* Various thoroughfare types serve the needs of pedestrians, motor vehicles, and bicycles (as required). A highway/walkway along the Wilson Creek Greenway Corridor will provide a link to the City of McKinney's trail system.

The **Regulating Plan** is a map showing the various transect-based zoning categories with precision. The regulating plan also shows the form and location of public spaces and the type and trajectories of thoroughfares.

The **Urban Standards** regulate the manner in which private buildings affect public spaces. These regulations are provided at multiple scales, from the entire thoroughfare section to the individual building configuration and frontage. The most comprehensive portion of the Urban Standards is the Building Type Summary. This diagrammatical chart plots individual Transect zones against Building Types to guide in the composition of the entire neighborhood. In addition to specifying which Types belong in which Transect zones, the Building Type Summary also provides guidelines for the layout of individual lots and building footprints.

The **Thoroughfare Standards** are an additional set of text and drawings/diagrams that directly relate to both the Regulating Plan and the Urban Standards. This information categorizes pedestrian and vehicular means of passage based on their capacity and urban character. The Thoroughfare Standards are illustrated in both plan and section, and specify characteristics such as vehicular travel and parking lanes, curbs, planters and planting strips, street trees, street lights, and pedestrian sidewalks.

The comprehensive **Architectural Standards** specify the materials and configurations deemed acceptable for the various building types in the Southern Land Community. They regulate criteria on multiple scales, from the facade, roof, and walls to the trim, finish, and hardware. The purpose of these regulations is to produce visual continuity and compatibility across the community and between different building types. On a large scale, the standards relate to the traditional vernacular of the region, in this case, northeast Texas. They also assure an appropriate architectural response to local climate and conditions. The importance of the Architectural Standards is their ability to create and maintain architectural harmony, which directly affects urban quality.

The Design Code is a series of prescriptions, some of which are mandatory and some of which are only recommended. The mandatory prescriptions are indicated by the verb shall. The recommended ones are indicated by the verb should. Options that are allowed but neither recommended or discouraged are indicated by the verb may.

The **Design Review Committee (DRC)** is a function of the developer, Southern Land Company, and the community's Homeowner's Association. The purpose of the DRC is to evaluate development plans and accompanying information submitted by qualified purchasers for adherence to the standards and criteria set forth in the Pattern Book Design Code and such other criteria and standards as may be promulgated by the DRC. The Committee shall interpret these standards and criteria, provide guidance to purchasers, builders, and designers, and may approve deviations and variances where special conditions or undue hardships arise. Approvals made through the Design Review Committee are not binding to City of McKinney, as City Ordinances take precedence over any other rules and regulations. Any deviations or variances from the Pattern Book Design Code or City of McKinney code requirements must receive approval from both the Design Review Committee and the City of McKinney Planning and Zoning Commission.

## TRANSECT ZONING

The term "Transect" is used to describe a cross-sectional system of classification of environments. All elements of the built environment are arranged in order from most rural to most urban. There are six transect zones, from Rural Preserve to Urban Core, all of which are described in detail on the following two pages.

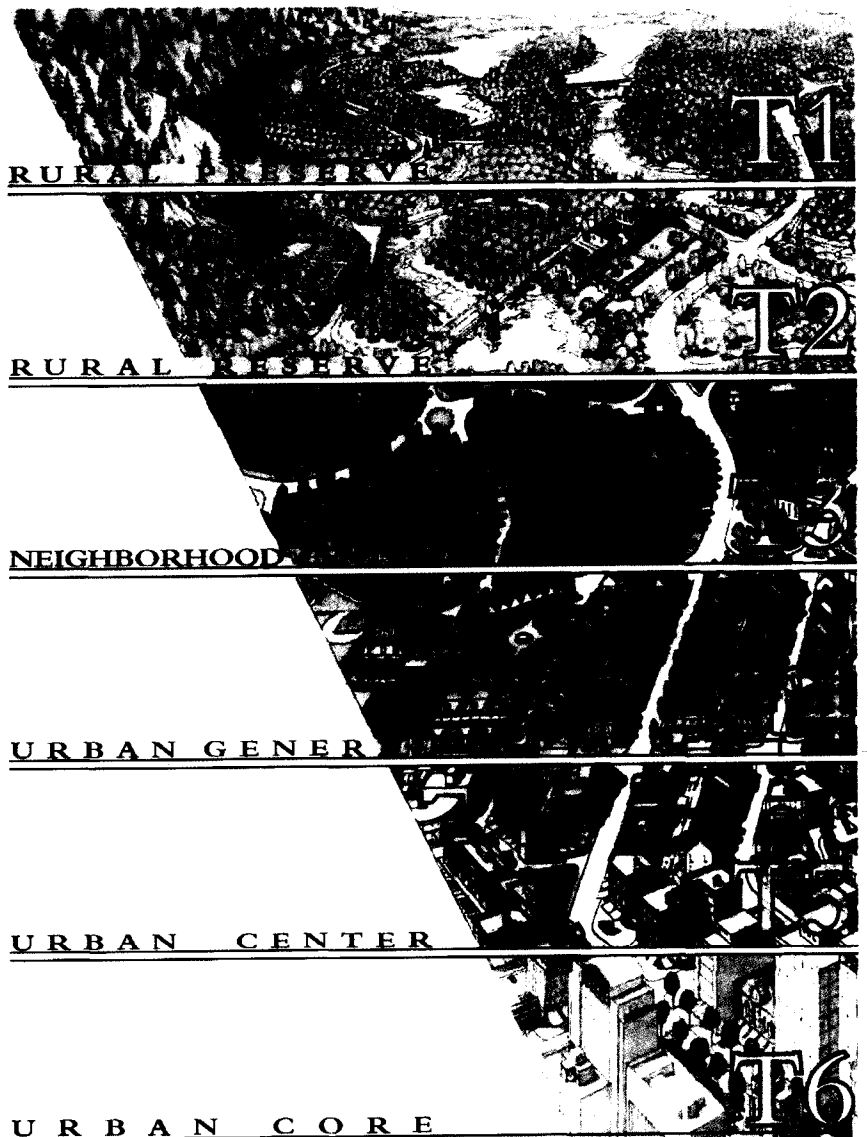
The image to the right shows the conceptual transect, increasing in density from top to bottom. Most master planned communities do not include all of the zones, but may include any number of them in any arrangement. As indicated, the Southern Land community is classified into the three middle zones: Neighborhood Edge (T3), Urban General (T4), and Urban Center (T5).

The purpose of the Transect system is to provide a carefully arranged alternative to conventional zoning and sprawl. While conventional zoning segregates building usage and functions, often by large distances, transect zoning allows for a mix of uses arranged in much closer proximity.

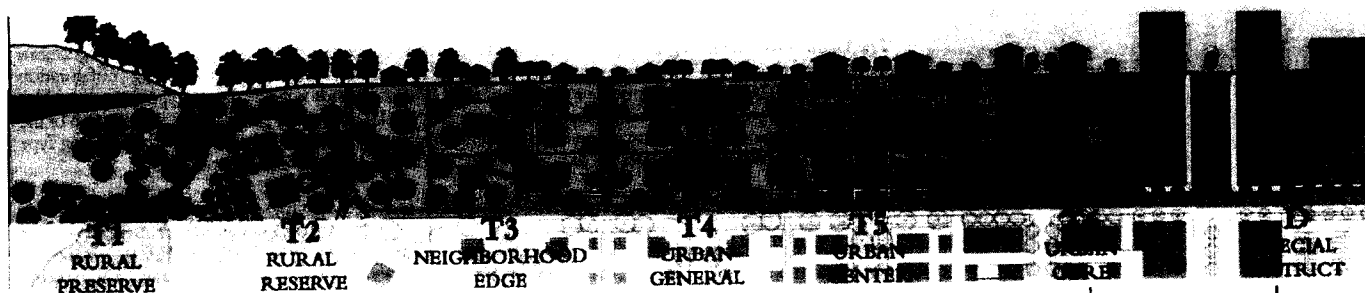
Transect zoning also offers a more qualitative approach to town planning by focusing on the character of a place. One key concept of the Transect system is the "immersive environment" in which all components create a harmonious place somewhere within the transect continuum. Each point along the continuum has specific unifying characteristics evident in everything that comprise it: from the street design to the building form, landscape, and public infrastructure. For example, in the T5 Urban Center zone, one might find parapet roofs instead of gabled, and four story buildings as opposed to two. All of these elements work together to create an intensely urban "immersive environment" that is distinctly different from its much more rural T2 counterpart.

Each transect zone has very specific requirements and provisions. Everything from density, thoroughfares, lot dimensions, public spaces, architectural massing, building frontages, building types, parking, pedestrian networks, and landscape design are quantified and specified in the transect descriptions and the accompanying design code. The charts and diagrams that follow detail these specific requirements and should serve to further clarify the Transect concept. Additional information may also be found in the Smart Code V7.0 document.

**CREDITS:** Transect definitions and illustrations at right courtesy of Smart Code V 7.0, Duany Plater-Zyberk & Company.







**T1 RURAL PRESERVE**  
This transect consists of lands approximating or reverting to a wilderness condition, including lands unsuitable for settlement due to topography, hydrology, or vegetation.

**T2 RURAL RESERVE**  
This transect consists of lands in open or cultivated states or sparsely settled. These may include woodland, agricultural lands, grasslands, and irrigable deserts.

**T3 NEIGHBORHOOD EDGE**  
This transect is similar to conventional low density suburban house areas but differs by allowing home occupations in accessory units. Planting is naturalistic with deep setbacks. Blocks may be large and the roads irregular to accommodate natural conditions.

**T4 URBAN GENERAL**  
This transect is a denser and primarily residential urban fabric. The transect has a wide range of building types including single, side yard, and row houses. Setbacks and landscaping are variable. Streets typically define medium-sized blocks.

**T5 URBAN CENTER**  
This transect is the equivalent of a downtown. It contains the tallest buildings, the greatest variety, and unique civic buildings in particular. It is the least naturalistic; street trees are steadily planted and sometimes absent.

**T5 URBAN CORE**  
This transect is the equivalent of a Main Street. It includes building types that accommodate retail, offices, residential and live/work units. It is usually a tight network of streets with wide sidewalks, steady street tree planting, and buildings set close to frontages.

**SPECIAL DISTRICT**  
These are the areas with buildings that, by their intrinsic function, disposition, or configuration, can not conform to one of the six normative Transect Zones. Typical districts may include institutional campuses, civic buildings, etc.

CREDITS: Transect illustrations and definitions courtesy of Smart Code V 7.0, Duany Plater-Zyberk & Company.

## GENERAL BUILDING TYPES BY TRANSECT

The Southern Land Community will be a mixed-use environment, with a combination of residences, retail spaces, offices, civic buildings, and open spaces, and a mix of three transect zones: T3, T4 and T5. Each of these uses will be located in close proximity, and often adjacent, to one another. In the residential areas, a variety of sizes and types of housing will be combined, often within a single block. In the neighborhood and mixed-use centers, this mix of uses will be both horizontal as well as vertical through the utilization of live/work townhouses and mixed-use multi-story commercial buildings. The building functions permitted within the Southern Land community are depended on the transect in which the building is located. While certain uses, such as single family detached dwellings, are allowed in any of the three transects, other uses may only be permitted in more urban transect zones.

For the purposes of this Design Code, the T5 transect has been further subdivided into T5 Residential Urban Centers and T5 Mixed Use Urban Centers. The Urban Standards and streetscape characteristics for each Urban Center are the same. The major difference being that Mixed-Use buildings are only permitted in Mixed-Use Urban Centers. Some of the T5 centers, while urban in character, are not intended to contain uses other than residential.

The following is a listing of permitted general building types by transect:

### T3: NEIGHBORHOOD CENTER

- \* Single family detached dwellings are permitted.
- \* Residential buildings containing not more than two dwelling units (shared wall houses) are permitted (townhomes are not permitted).
- \* Only one building for living purposes shall be permitted on one zoning lot unless otherwise indicated.
- \* Accessory units (ex: Carriage Houses) no greater than 1,000 square feet may be permitted on the same lot as a single family detached dwelling. These units may be used as rental property.
- \* Bed and Breakfast type lodging may be allowed by special use permit.

### T4: URBAN GENERAL

- \* Single family detached dwellings are permitted.
- \* Residential buildings containing not more than two dwelling units (shared wall houses) are permitted (townhomes are not permitted).
- \* Only one building for living purposes shall be permitted on one zoning lot unless otherwise indicated.
- \* Accessory units (ex: Carriage Houses) no greater than 1,000 square feet may be permitted on the same lot as a single family detached dwelling. These units may be used as rental property.
- \* Bed and Breakfast type lodging may be allowed by special use permit.
- \* Civic Buildings are permitted.

### T5: RESIDENTIAL URBAN CENTER:

- \* Single family detached dwellings are permitted.
- \* Residential buildings containing two dwelling units (shared wall houses) are permitted.
- \* Residential buildings containing side-by-side townhouse units are permitted.
- \* Accessory units (ex: Carriage Houses) no greater than 1,000 square feet may be permitted on the same lot as a single family detached dwelling. These units may be used as rental property.
- \* Live/Work buildings containing single family attached townhouse units above commercial retail/office space are permitted.
- \* Bed and Breakfast type lodging is permitted.
- \* Civic Buildings are permitted.

### T5: MIXED-USE URBAN CENTER:

- \* Single family detached dwellings are permitted.
- \* Residential buildings containing two dwelling units (shared wall houses) are permitted.
- \* Residential buildings containing horizontally stacked townhouse units are permitted.
- \* Accessory units (ex: Carriage Houses) no greater than 1,000 square feet may be permitted on the same lot as a single family detached dwelling. These units may be used as rental property.
- \* Live/Work buildings containing single family attached townhouse units above commercial retail/office space are permitted.
- \* Bed and Breakfast type lodging is permitted.
- \* Civic Buildings are permitted.
- \* Mixed-use buildings are permitted, however, residential uses are not permitted on first floors.





DESIGN CODE  
PERMITTED USES BY TRANSECT

The building functions deemed appropriate within the Southern Land community are generally more restrictive than within the City of McKinney. The following residential, commercial, and civic uses are considered to be in keeping with the desired quality and characteristics of the community and are permitted in all transects unless otherwise indicated:

Primary Uses (Entire community):

- \* Single-family detached dwellings.
- \* Public, parochial, and private schools offering courses of general instruction.
- \* Churches, synagogues, chapels, and similar places of worship located in a permanent structure.
- \* Public and quasi-public buildings for cultural use (museums, art galleries, etc)
- \* Bed and Breakfast establishments as defined by City of McKinney standards. Bed and Breakfasts are allowed in any T5 zone, but shall require a Special Use Permit (SUP) to be operated in T3 and T4.
- \* Utility substations necessary to the functioning of the utility, but not including general business offices, maintenance facilities, and other general system facilities.
- \* Parks and recreation areas operated by the City of McKinney
- \* Uses similar to the above mentioned permitted uses, provided activities conducted observe the requirements of all City ordinances.

Accessory Uses (Entire community):

Accessory uses are permitted in all three transects as per City of McKinney Zoning Ordinance Sec. 41-68 (RS Single-Family Residence District Regulations).

- \* Home occupations
- \* Private garages and parking areas
- \* Private swimming pools for the use of residents and their nonpaying guests
- \* The parking of one unoccupied recreational trailer no more than 24 feet in length; trailer shall not be visible from the public right-of-way
- \* The storage of one recreational boat either in a building or in the rear yard; boat shall not be visible from the public right-of-way
- \* The keeping of dogs, cats, and other household pets
- \* Other accessory uses and buildings appurtenant to a permitted use, including, but not limited to, associational meetings, religious gatherings, and social activities

Within the T5 Urban Centers (Residential and Mixed-Use), Live/Work buildings containing townhouse units above commercial/office space are permitted. The non-residential uses allowed differ depending on the categorization of the Urban Center. Live/Work units within the Residential Urban Center are more limited than those within the Mixed Use Urban Center. The following non-residential uses are considered appropriate for any Live/Work unit in the Southern Land community:

- |                              |                                    |
|------------------------------|------------------------------------|
| * Bed and Breakfast          | * Furniture Sales                  |
| * Antique Shop               | * Hobby or Handcraft Shop          |
| * Apparel and Accessory Shop | * Massage therapist                |
| * Art Supply Shop            | * Natural healing practitioner     |
| * Barber or Beauty Shop      | * Office Supply Store              |
| * Book, Card, or Gift Shop   | * Restaurant or cafeteria (indoor) |
| * Fabric or Needlework Shop  | * Retail Store (indoor)            |
| * Florist or Garden Shop     | * Studio (photography, arts)       |
| * Grocery or Food Store      | * Travel Agency                    |

In addition to Live/Work units, Mixed-Use commercial buildings are a permitted building type within the T5 Mixed Use Urban Centers. The following non-residential uses, in addition to all uses listed above, are considered appropriate for any Mixed Use commercial building within a T5 Mixed Use Center:

All Principal Permitted Uses allowed by City of McKinney Zoning Ordinance Sec. 41-79 BN Neighborhood Business District Regulations with the following additions and exceptions:

Additional allowable uses:

- \* Theater (indoor)
- \* Department or discount store
- \* Furniture sales
- \* Office supplies
- \* Veterinarian (no outside runs)

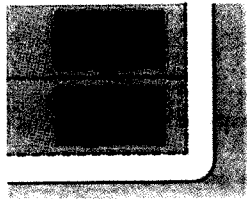
Exceptions to allowable uses:

- \* No boarding houses or rooming houses
- \* No multi-family dwellings (apartments)
- \* No colleges or universities
- \* No fraternal organizations, lodges, or civic clubs (with the exception of HOA Residents' Clubs)
- \* No halfway houses
- \* No hospitals



## BUILDING DISPOSITION

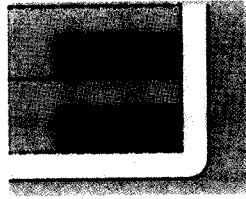
### Edge Yard



T1 T2 T3 T4

A building that occupies the center of its lot with setbacks on all sides. This is the least urban of all types, as the front yard sets it back from the frontage, while the side yards weaken the spatial definition of the public thoroughfare space. The front yard is intended to be visually continuous with the yards of adjacent buildings. The rear yard can be secured for privacy by a fence/wall and a well-placed back building or outbuilding.

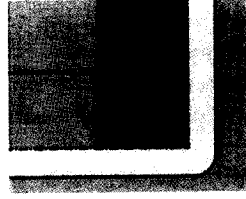
### Side Yard



T4 T5

A building that occupies one side of the lot with the setback to the other side. The visual opening of the side yard on the street frontage causes this building type to appear freestanding. A shallow frontage setback defines a more urban condition. If the adjacent building is similar with a blank party wall, the yard can be quite private. This type permits systematic climatic orientation in response to the sun or the breeze.

### Rear Yard



T5

A building that occupies the full frontage, leaving the rear of the lot as the sole yard. This is a very urban type as the continuous facade steadily defines the public thoroughfare. The rear elevations may be articulated for functional purposes. In its residential form, this type is the row house. For its commercial form, the rear yard can accommodate substantial parking.

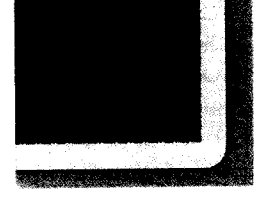
### Court Yard



T5 T6

A building that occupies the boundaries of its lot while internally defining one or more private patios. This is the most urban of types, as it is able to shield the private realm from all sides while strongly defining the public thoroughfare. Because of its ability to accommodate incompatible activities, masking them from all sides, it is recommended for workshops, lodging, and schools. The high security provided by the continuous enclosure is useful for crime-prone areas.

### Specialized



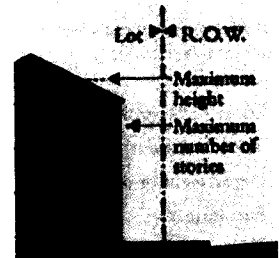
### Special District

A building that does not fit the building dispositions generally found in the transects. Buildings that fit within the special district would be civic buildings, churches, and/or amenity centers. Approval of building disposition for the special district will occur with site plan approval. Determination of the building disposition will be made by whether or not the building is in keeping with the principles of the Pattern Book as well as the goals and objectives of the City of McKinney Comprehensive Plan and Zoning Ordinance. The Planning and Zoning Commission and/or Staff may deny the site plan if it is found to not be in keeping with these.

*CREDITS: Building Disposition definitions and illustrations courtesy of Smart Code V 7.0, Duany Plater-Zyberk & Company.*

## BUILDING HEIGHT:

- \* Building Height has been defined by the International Building Code as "vertical distance from grade plane to the average height of the highest roof surface" and shall be measured in number of feet.
- \* Maximum principal building height (as indicated on the following charts) is to be measured in both feet, as defined above, and in stories. A residential story shall not exceed twelve (12) feet from floor to floor, not including a raised basement or an inhabited attic. A retail story shall be between twelve (12) and eighteen (18) feet from floor to floor. A half story is defined as an inhabited space beneath a gabled roof and therefore varying in floor to ceiling height.
- \* "Bungalow" type lots (those that front on pedestrian thoroughfares instead of vehicular thoroughfares) may be limited to 30' in height. City of McKinney fire code stipulates that all buildings taller than 30' require access by a 26' or wider fire lane.
- \* A portion of the building no more than 250 square feet (tower, turret, cupola, steeple, chimney, etc) may exceed the height limit indicated for each building type on the subsequent pages. However, no portion of any building in any transect may exceed seventy five (75) feet in height, as subject to approval by the Design Review Committee (DRC) and the City of McKinney Planning and Zoning Commission.



## STREETSCAPE STANDARDS:

The following issues should be considered when siting a building:

- \* The streetscape shall be a continuous edge, using a combination of building faces, low walls, canopy structures, fences, screening walls around service areas, street furniture, and/or landscaping. Blank facades in excess of 40 linear feet shall not be exposed to public thoroughfares.
- \* Screening is required for all storage, service portals, garbage areas, and service driveways visible from public streets as per City of McKinney Zoning Ordinance Sec. 41-205 "Fences, Walls, and Screening Requirements."
- \* Blocks shall not exceed 800 feet in length in T3, 600 feet in length in T4, and 400 feet in length in T5. If blocks exceed these lengths, alleys or pedestrian paths should be provided for through access.
- \* All buildings shall have an identifiable public address that corresponds to the main entrance of the building.
- \* Garages accessed from the front of the lot shall be set back a minimum of 20 feet from the back of the house or rotated so that the garage doors do not face adjacent streets.
- \* On non-alley homes, front-facing garages should be twenty (20) feet behind the primary building.





## DESIGN CODE

### BUILDING TYPE SUMMARY INTRODUCTION

The **Building Type Summary** provides standards for the placement and massing of each of the nine building types in the Southern Land community.

The top row of the chart contains basic information about each building type, including building function, frontage type, building disposition, and drive access.

The chart itself specifies the types of buildings that may occur in each of the three Transect zones in the community. A blank space in the chart indicates that that particular building type is not permitted in the transect.

Each segment of the chart contains a diagram and all of the pertinent dimensions and specifications for building placement and height within a given lot. Note that many of these values are provided as a range of dimensions.

It should be noted that while the majority of the buildings within the Southern Land community fall within the parameters of the Building Types detailed on the following pages, certain buildings may not. These buildings are those that fall under the "Special District" and "Specialized" Building Disposition categories described on the previous pages. An example of such a building may be a school or a community center on a unique lot, or a residential lot in a unique, non-rectangular shape due to site conditions. In such cases, the building and the lot must still adhere to all of the requirements set forth in the Pattern Book, in addition to all of the requirements set forth in the City of McKinney Zoning Ordinance. Final approval on all such lots shall be given by the Planning and Zoning Commission and is dependent on whether or not the building is in keeping with the principles of the Pattern Book as well as the goals and objectives of the City of McKinney Comprehensive Plan and Zoning Ordinance. The Planning and Zoning Commission and/or Staff may deny the site plan if it is found to not be in keeping with these.

Following the Building Type Summary are several pages of Lot Configuration Options for each of the seven different residential building types. This section illustrates different methods of fulfilling minimum parking requirements on each lot type.

### ADDITIONAL CRITERIA FOR CARRIAGE HOUSES:

The following criteria applies to Carriage Houses as regulated within the Building Type Summary: Residential chart. Carriage Houses are categorized as Accessory Units and are subject to the "Permitted Uses by Transect" outlined in the Building Function section of the code. Residential uses within Carriage Houses are subject to the approval of the City of McKinney Fire Department.

- \* Corner carriage units directly accessible from a side street may be habitable.
- \* Interior carriage units not directly accessible from a fire lane are prohibited from being used as dwelling units, unless adequately sprinklered and fire-protected as determined by the City of McKinney Fire Department.

### TERMS AND DEFINITIONS:

**Frontage Line:** A lot line that coincides with a right-of-way or a public space.

**Facade:** An exterior building wall which faces a frontage line. Facades shall be set back from the frontage and lot lines as shown in the diagram. Facades shall be set parallel to straight frontage lines, and parallel to the chord if frontage line is broken or curved.

**Elevation:** Any exterior building wall not facing a frontage. Elevations shall be set back from the frontage and lot lines as shown in the diagram. Elevations may deviate from the trajectory of the lot lines.

**Front Setbacks:** The distance between the frontage line and a facade. **Front setbacks** are intended to be *fixed* "Build-To" lines for the main body of the house. Deviation from the setback requirement shall occur at a minimum and is subject to DRC and City Planning and Zoning Commission approval. **Encroachments** will provide the variety in the streetscape that is intended and desired by the developer.

**Encroachments:** A building element permitted to extend beyond the allowable building setback. Encroachments by stoops, porches, bay windows, canopies, awnings, and balconies within the area between the sidewalk and the building facade are permitted.

**Rear Setback:** The distance between the rear lot line and any portion of the main body of the building. **Rear Setbacks** are *recommended* "Build-To" lines for the main body of the house. On lots with attached garages, a fewer-story connector or breezeway may occur within the setback. Deviation from the setback requirement shall occur at a minimum and is subject to DRC and City Planning and Zoning Commission approval.

**Side Setback:** The distance between the side lot line and an elevation of the building with the exception of roof overhangs. **Side Setbacks** are intended to be *fixed* "Build-To" lines for the main body of the house. Encroaching elements as described above are permitted to extend beyond the allowable building setback on the street side of corner lots. Otherwise, chimneys are the only building element that may extend into a side setback. Deviation from the setback requirement shall occur at a minimum and is subject to DRC and City Planning and Zoning Commission approval.

The side setback on the street side of a corner lot is dependent on the transect in which the lot is located. Side setbacks for corner lots are as follows:

T3 Neighborhood Edge:	Three (3) feet minimum
T4 Urban General:	Eight (8) feet minimum
T5 Urban Center:	Thirteen (13) feet minimum





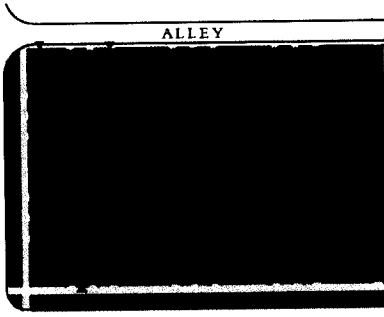
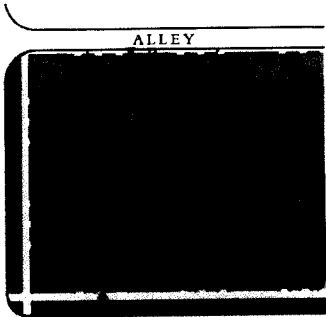
LIVE/WORK UNIT

- \* Rear yard building type
- \* May have combination of residential, retail, and/or office uses
- \* Non-residential (commercial and/or office) use on ground floor minimum (may take up entire ground floor and/or part of upper floors)
- \* Upper floors may have any combination of office, single-family attached residential, and support uses.
- \* Detached building on own lot
- \* May have one ancillary structure
- \* Alley drive access

MIXED-USE BUILDING

- \* Rear yard building type
- \* Shared lot or separate lots with common walls
- \* May contain a single commercial or office use, or a combination of commercial, office, and residential.
- \* Non-residential (commercial and/or office) use on ground floor minimum (may take up entire ground floor and/or part of upper floors)
- \* Upper floors may have any combination of office, residential, and support uses.
- \* Detached building on own lot
- \* May have one ancillary structure

T5  
URBAN  
CENTER



LOT SIZE:	(Width x Depth)
SETBACKS:	(Primary Front/Rear)
	(Primary and Outbuilding Side)
	(Outbuilding Rear)
ENCROACHMENTS:	(Building front)
	(Building side)
HEIGHT:	(Principal building)
	(First floor above grade)
	(Back building and outbuilding)

20' min. x 80' min.
5' min./60' min.
0' min. (10' between buildings) and 3' min. on corners
5' min.
5' max. (must not exceed frontage line)
1.5' max. (for upper stories only)
2 stories min./ 3.5 stories max.
12' min. (18" recommended)
4 stories max.

NOTE: Live/Work building types may occur in any T5 Urban Center

18-144' x 60' min.
5' min./0' min.
0'-20' max. (10' between buildings) and 3' min. on corners
0' min.
5' max. (must not exceed frontage line)
1.5' max. (for upper stories only)
2 stories min./ 4 stories max.
12' min. (18" recommended)
4 stories max.

NOTE: Mixed-Use building types may only occur in T5 Mixed Use Urban Centers.





DESIGN CODE

BUILDING TYPE SUMMARY: RESIDENTIAL

NOTE: Building types shown are considered to be typical lot configurations. Garages on any single-family lot may be either detached (as shown in the illustrations) or attached by way of an enclosed connector or a partially enclosed breezeway. Additional lot configuration options are provided on the following pages.

- \* Edge yard building type
- \* Single family dwelling
- \* Detached building on own lot
- \* One or more ancillary structures.
- \* Between 2,800 and 4,700 SF of conditioned space and 2.5 stories max., depending on Transect location.
- \* Non-alley drive access

NON-ALLEY HOUSE

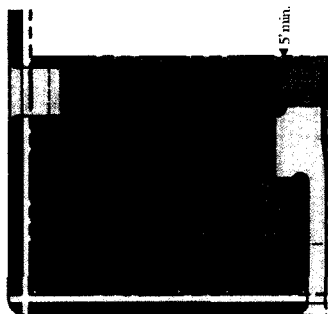
- \* Edge yard building type
- \* Single family dwelling
- \* Detached building on own lot
- \* May have ancillary structures.
- \* Between 2,500 and 4,300 SF of conditioned space and 3.5 stories max., depending on Transect location
- \* Non-alley drive access

ALLEY HOUSE

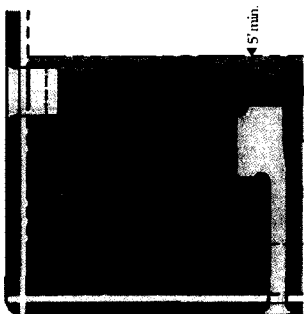
- \* Edge yard building type
- \* Single-family dwelling
- \* Detached building on own lot
- \* May have ancillary structures.
- \* Between 1,900 and 4,500 SF of conditioned space and 3 stories max., depending on Transect location.
- \* Alley drive access

T3  
NEIGHBORHOOD  
EDGE

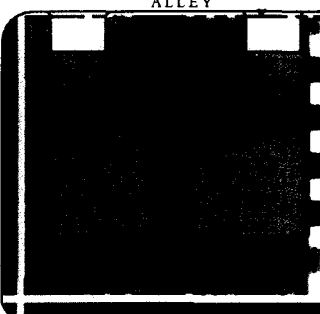
LOT SIZE:	(Width x Depth)
SETBACKS:	(Primary Front/Rear)
	(Primary and Outbuilding Side)
	(Outbuilding Rear)
ENCROACHMENTS:	(Building front)
	(Building side)
HEIGHT:	(Principal building)
	(First floor above grade)
	(Back building and outbuilding)



85' min. x 110' min.  
28' / 45' min.



55' min. x 110' min.  
28' / 45' min.

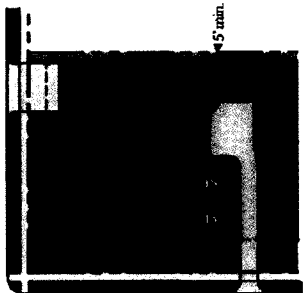


50' min. x 110' min.  
28' / 45' min.

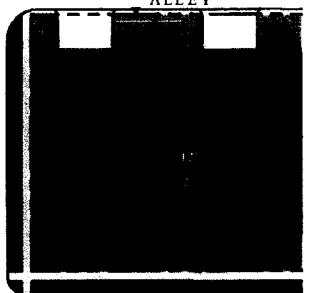
3' min. (21' between buildings); 13' on corners	5' min. (21' between buildings); 13' on corners	5' min. (15' between buildings); 13' on corners
5' min.	5' min.	5' min.
12' max.	12' max.	10' max.
8' max.	6' max.	2' max.
50' or 2.5 stories max.	50' or 2.5 stories max.	50' or 2.5 stories max.
12' min. (18" recommended)	12' min. (18" recommended)	12' min. (18" recommended)
2 stories max.	2 stories max.	2 stories max.

T4  
URBAN  
GENERAL

LOT SIZE:	(Width x Depth)
SETBACKS:	(Primary Front/Rear)
	(Primary and Outbuilding Side)
	(Outbuilding Rear)
ENCROACHMENTS:	(Building front)
	(Building side)
HEIGHT:	(Principal building)
	(First floor above grade)
	(Back building and outbuilding)



55' min. x 100' min.  
18' / 45' min.

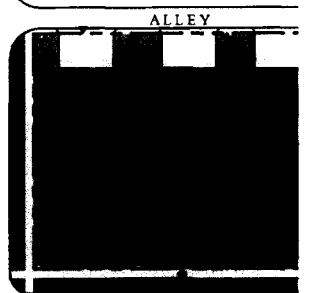


40' min. x 100' min.  
18' / 50' min.

3' min. (18' between buildings); 8' on corners	5' min. (12' between buildings); 8' on corners
5' min.	5' min.
10' max.	10' max.
6' max.	2' max.
60' or 3.5 stories max.	60' or 3 stories max.
12' min. (18" recommended)	12' min. (18" recommended)
2.5 stories max.	2 stories max.

T5  
URBAN  
CENTER

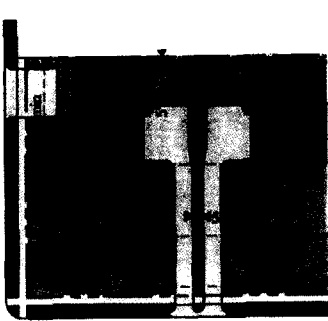
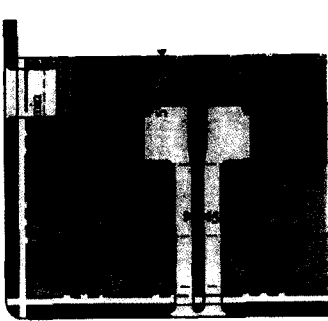
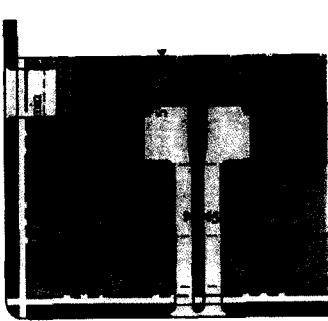
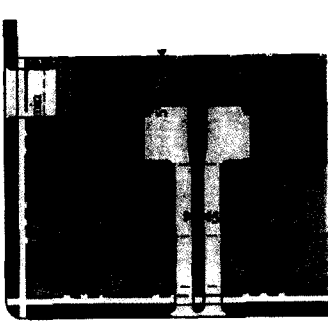
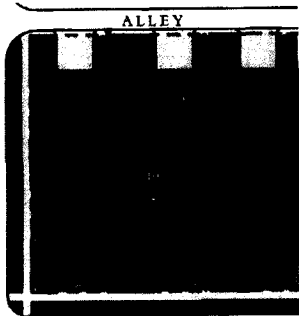
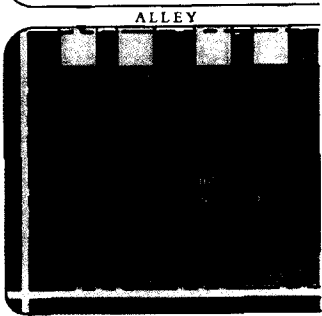
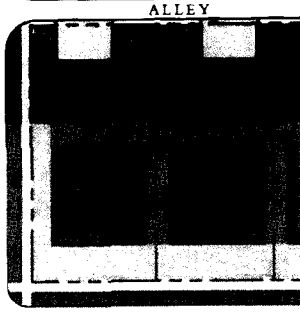
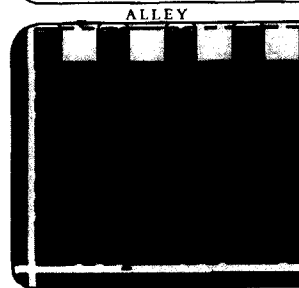
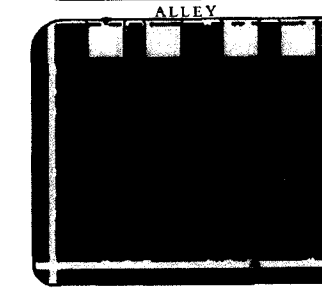
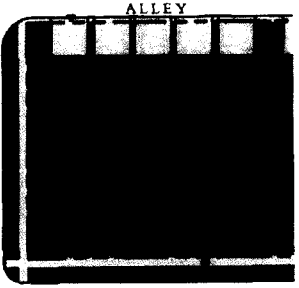
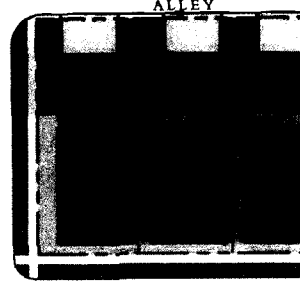
LOT SIZE:	(Width x Depth)
SETBACKS:	(Primary Front/Rear)
	(Primary and Outbuilding Side)
	(Outbuilding Rear)
ENCROACHMENTS:	(Building front)
	(Building side)
HEIGHT:	(Principal building)
	(First floor above grade)
	(Back building and outbuilding)



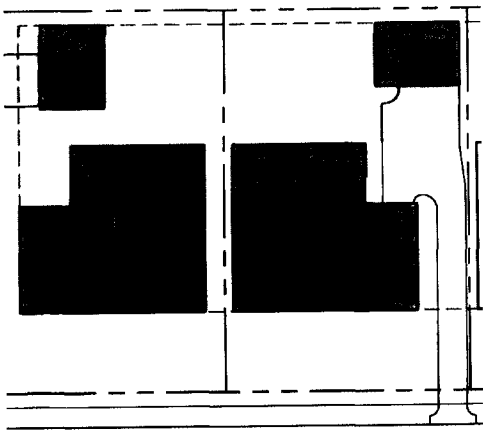
35' min. x 50' min.  
5' / 45' min.

3' min. (10' between buildings); 3' on corners
5' min.
5' max. (must not exceed frontage line)
2' max.
60' or 3 stories max.
12' min. (18" recommended)
2.5 stories max.



COTTAGE	SHARED WALL HOUSE	TOWNHOUSE	CARRIAGE HOUSE
<ul style="list-style-type: none"><li>* Edge yard building type</li><li>* Single-family dwelling</li><li>* Detached building on own lot</li><li>* Between 1,900 and 2,500 SF of conditioned space and 2.5 stories max., depending on Transect location.</li><li>* Alley drive access</li></ul>	<ul style="list-style-type: none"><li>* Rear yard building type</li><li>* Single-family paired dwelling.</li><li>* Attached building on separate (common walls between units) or shared lot.</li><li>* Facades form continuous frontage line</li><li>* Between 1,800 and 2,400 SF of conditioned space and 3 stories max., depending on Transect location.</li><li>* Alley or non-alley drive access</li></ul>	<ul style="list-style-type: none"><li>* Rear yard or no yard building type</li><li>* Single-family attached dwellings</li><li>* Attached building on separate lots</li><li>* Common walls on side lot lines</li><li>* Between 1,500 and 2,200 SF of conditioned space and 4 stories max., depending on Transect location.</li><li>* Alley drive access</li></ul>	<ul style="list-style-type: none"><li>* Rear yard building type</li><li>* Single-family dwelling* or accessory unit **</li><li>* May be on separate or shared lot.</li><li>* Up to 1,000 SF of conditioned space and 2.5 stories max. above garage, depending on Transect location.</li><li>* Alley or non-alley drive access</li></ul>
			
	30 min. x 110' min. 28'/50' min. Attached or 10' min. between buildings; 13' on corners 5' min. 10' max. 0' on one side, 2' max. on other side 50' or 2.5 stories max.* 12" min. (18" recommended) 2 stories max.		30' min. x 30' min. N/A or 5' min. 3' min. N/A 1' max. 1' max. 1.5 stories max. (excludes Garage) 12" min. (18" recommended) N/A
			
35' min. x 100' min. 18'/50' min. 3' min. (10' between buildings); 8' on corners 5' min. 10' max. 1' max. 50' or 2.5 stories max.* 12" min. (18" recommended) 2 stories max.	30' min. x 100' min. 18'/50' min. Attached or 10' min. between buildings; 8' on corners 5' min. 10' max. 0' on one side, 2' max. on other side 50' or 2.5 stories max.* 12" min. (18" recommended) 2.5 stories max.		25' min. x 25' min. N/A or 3' min. 3' min. N/A 1' max. 1' max. 1.5 stories max. (excludes Garage) 12" min. (18" recommended) N/A
			
30' min. x 50' min. 5'/50' min. 3' min. (10' between buildings); 3' on corners 5' min. 5' max. (must not exceed frontage line) 1' max. 50' or 2 stories max.* 12" min. (18" recommended) 2 stories max.	30' min. x 50' min. 5'/60' min. Attached or 10' min. between buildings; 3' on corners 5' min. 5' max. (must not exceed frontage line) 1' max. 60' or 3 stories max.* 12" min. (18" recommended) 2.5 stories max.	18' min. x 50' min. 5'/60' min. Attached or 10' min. between buildings; 3' on corners 5' min. 5' max. (must not exceed frontage line) 2' max. at corner lots 4 stories max. 12" min. (18" recommended) 3.5 stories max.	25' min. x 25' min. N/A or 3' min. 3' min. N/A 1' max. 1' max. 2.5 stories max. (excludes Garage) 12" min. (18" recommended) N/A
* "Bungalow" type lots may be limited to 30' in height. City of McKinney fire code stipulates that all buildings taller than 30' require access by a 26' or wider fire lane.	* Shared Wall Houses are not permitted to be stacked one on top of another. Residential units may be side-by-side only.		* See criteria on Building Type Summary Introduction page. ** See "Accessory Uses" section on Permitted Uses by Transect page.





Typical Layout

### VILLA LOTS

#### Parking Requirements (per dwelling unit):

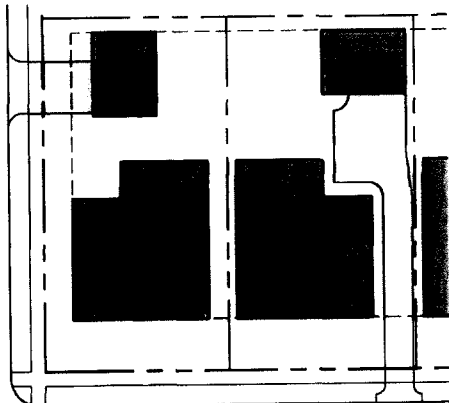
- \* Minimum two-car garage (or covered carport) with two exterior parking spaces.
- \* Garage may be attached or detached.

#### Typical Parking Layout (shown at left):

- \* Two-car front-loaded street-facing garage with standard parking pad. (Two garage parking spaces and two exterior parking spaces per unit).

#### Parking Options (shown at right):

- A: Midblock Lot; two-car front-loaded side-facing garage with standard parking pad. (Two garage parking spaces and two exterior parking spaces.)
- B: Corner lot; two-car side-loaded rear-facing garage with standard parking pad and turnaround.
- C: Corner lot; two-car side-loaded side-facing garage with standard parking pad.



Typical Layout

### NON-ALLEY LOTS

#### Parking Requirements (per dwelling unit):

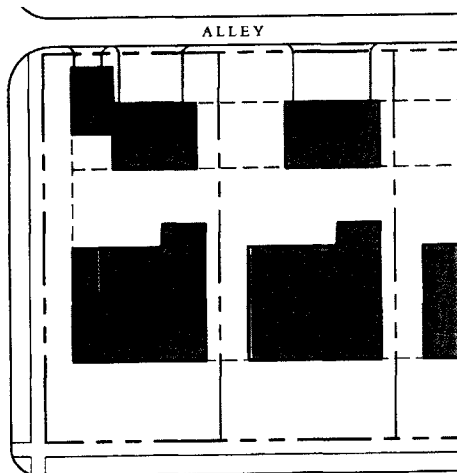
- \* Minimum two-car garage (or covered carport) with two exterior parking spaces.
- \* Garage may be attached or detached.

#### Typical Parking Layout (shown at left):

- \* Two-car front-loaded street-facing garage with standard parking pad. (Two garage parking spaces and two exterior parking spaces per unit).

#### Parking Options (shown at right):

- A: Midblock Lot; two-car front-loaded side-facing attached garage with standard parking pad. (Two garage parking spaces and two exterior parking spaces.)
- B: Corner lot; two-car side-loaded rear-facing garage with standard parking pad and turnaround.
- C: Corner lot; two-car side-loaded side-facing garage with standard parking pad.



Typical Layout

### ALLEY LOTS

#### Parking Requirements (per dwelling unit):

- \* Minimum two-car garage (or covered carport) with two exterior parking spaces.
- \* Garage may be attached or detached.

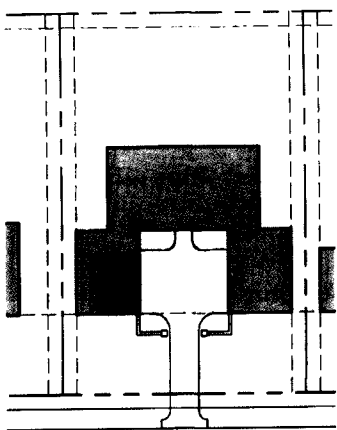
#### Typical Parking Layout (shown at left):

- \* Two to three-car alley-loaded garage (or covered carport) with standard parking pad. (one to three garage parking spaces and two exterior parking spaces per unit).

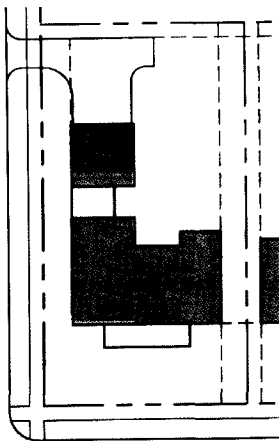
#### Parking Options (shown at right):

- A: Two-car alley-loaded garage with standard parking pad (two garage parking space and two exterior parking spaces per unit). On corner units, half or all of garage may be pulled back as far as allowable to enclose alley entrance and side-saddle parking pad. On mid-block units, parking pad not required to be side-saddle.
- B: Two to three-car garage with standard parking pad (two to three garage parking spaces and two exterior parking spaces per unit). One garage bay may be pulled forward as long as room is provided for at least two exterior spaces.

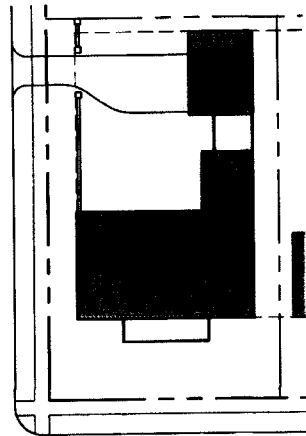




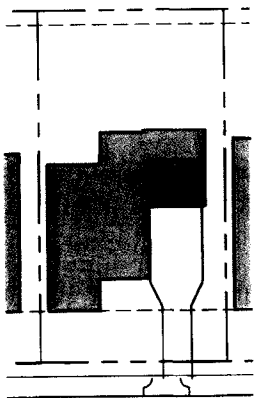
Option A



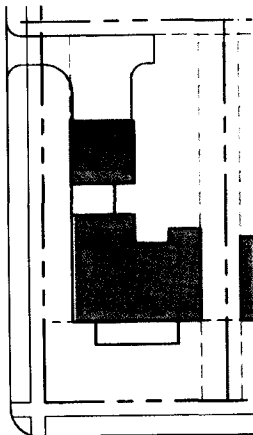
Option B



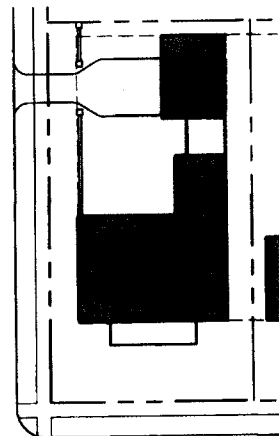
Option C



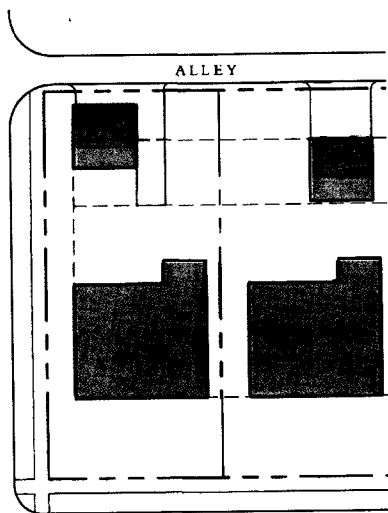
Option A



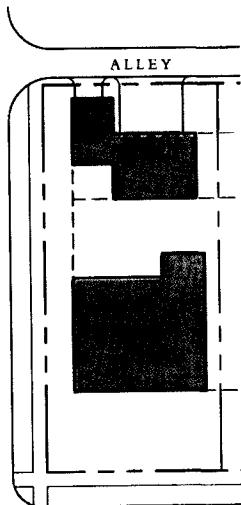
Option B



Option C

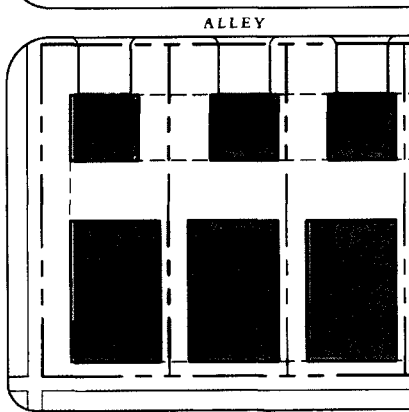


Option A



Option B





Typical Layout

COTTAGE LOTS

Parking Requirements (per dwelling unit):

- \* Minimum two-car garage (or covered carport) with two exterior parking spaces.
- \* Garage may be attached or detached.

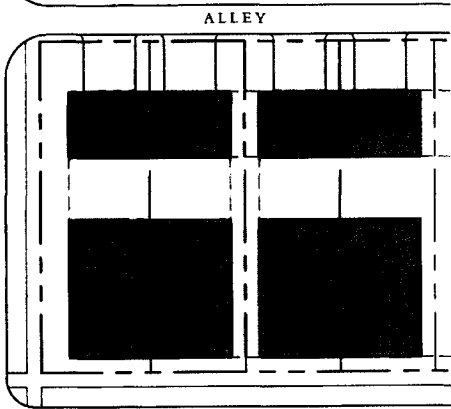
Parking Options:

- A: Two-car alley-loaded garage with standard parking pad (two garage parking space and two exterior parking spaces per unit). On corner units, half or all of garage may be pulled back as far as allowable to enclose alley entrance and side-saddle parking pad. On mid-block units, parking pad not required to be side-saddle.

Exceptions to Parking Requirements (subject to approval by the Planning and Zoning Commission):

- \* On cottage lots, the minimum requirement shall be reduced if either of the following conditions apply:
  - Lots are alley-loaded with a surface parking lot directly adjacent on the other side of the alley. OR
  - Lots are within 300 feet of a parking enclave containing no less than one (1) parking space for every two (2) lots with reduced parking (less than City of McKinney requirements).

If these either of these conditions apply, the parking requirement shall be reduced to one (1) garage (or covered carport) with two (2) exterior parking spaces.



Typical Layout

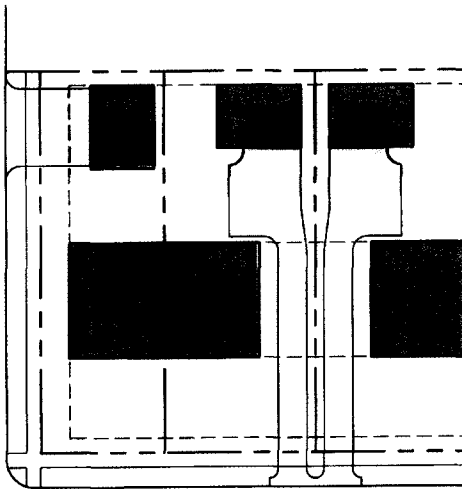
SHARED WALL HOUSE LOTS: ALLEY LOADED

Parking Requirements (per dwelling unit):

- \* Minimum one-car garage (or covered carport) with two exterior parking spaces.
- \* Garage may be attached or detached.

Parking Options:

- A: Double shared garage with standard parking pad or side saddle parking pads on either side (one exterior parking space and one garage parking space per unit). On corner lots the garage is pulled back as far as allowable to enclose alley entrance. More than one exterior parking space may be provided as space allows.
- B: One-car garage with one-car standard parking pad or one side saddle parking pad (one exterior parking space and one garage parking space per unit). On corner lots garage is pulled back as far as allowable to enclose alley entrance. More than one exterior parking space may be provided as space allows.



Typical Layout

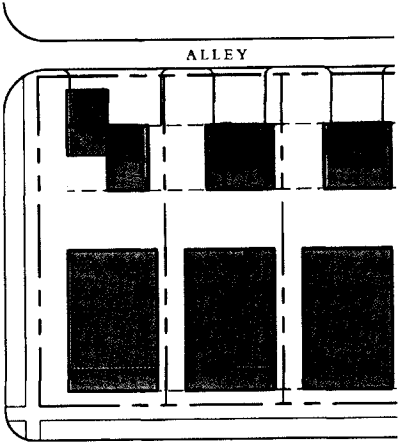
SHARED WALL HOUSE LOTS: NON-ALLEY LOADED

Parking Requirements (per dwelling unit):

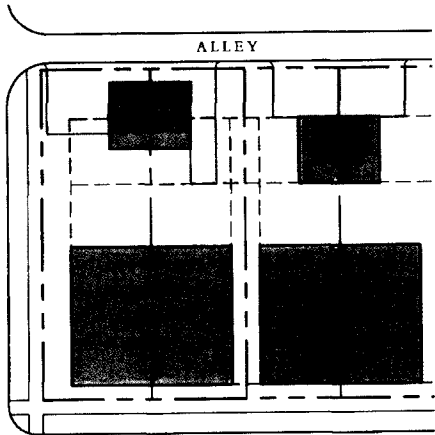
- \* Minimum one-car garage (or covered carport) with two exterior parking spaces.
- \* Garage may be attached or detached.

Parking Options:

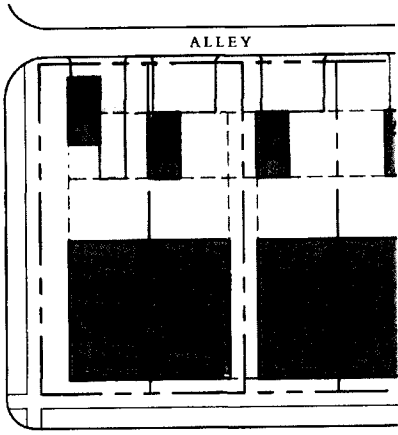
- A: Back-to-back attached garages and paired side-by-side driveways with standard parking pads (two exterior parking spaces and two garage parking spaces per unit). Additional exterior parking spaces may be provided as space allows.
- B: Double shared garage with separate double parking pads for each unit (two exterior parking spaces and one garage parking space per unit).



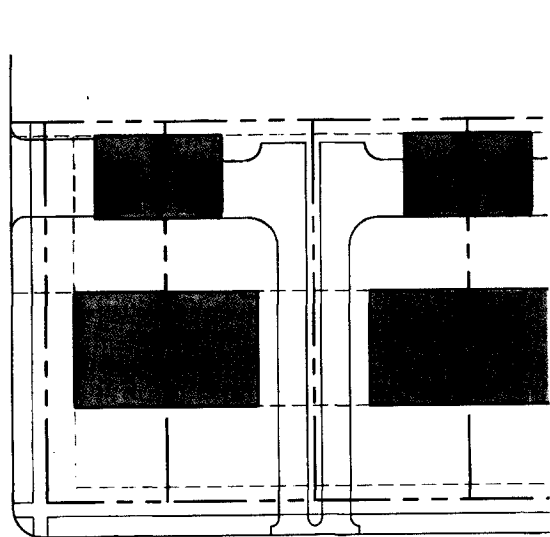
Option A



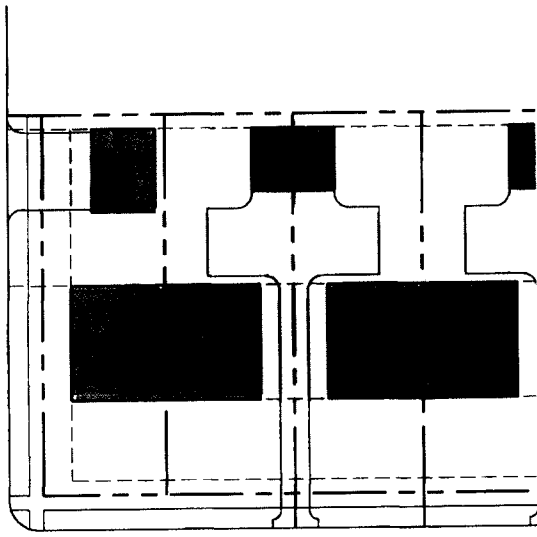
Option A



Option B

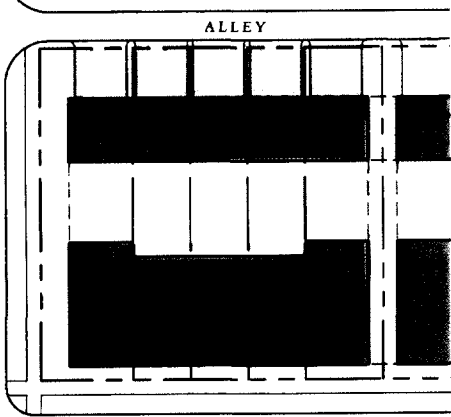


Option A



Option B





Typical Layout

TOWNHOUSE LOTS

Parking Requirements (per dwelling unit):

- \* Minimum two-car garage (or covered carport) with two (2) exterior parking spaces.
- \* Garage may be attached or detached.

Exceptions to Parking Requirements (subject to approval by the Planning and Zoning Commission):

- \* On townhouse lots, the minimum requirement shall be reduced if either of the following conditions apply:
  - Townhouse lots are alley-loaded with a surface parking lot directly adjacent on the other side of the alley. OR
  - Townhouse lots are within 300 feet of a parking enclave containing no less than one (1) parking space for every two (2) townhouses with reduced parking (less than City of McKinney requirements).

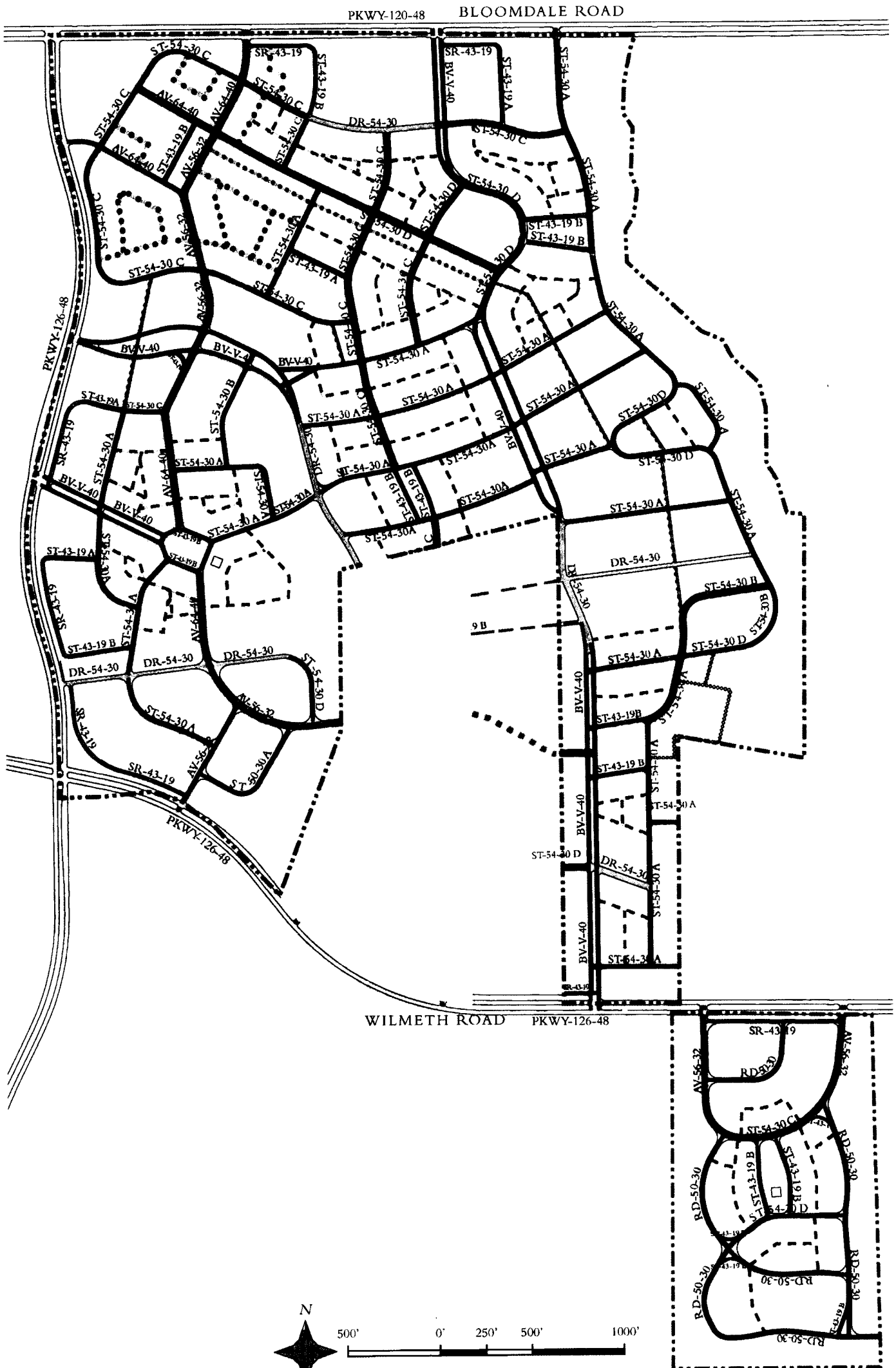
If these either of these conditions apply, the parking requirement shall be reduced to one (1) garage (or covered carport) with two (2) exterior parking spaces.

*NOTE: On townhouse lots less than 22' in width, there will not be adequate space for a two-car garage, therefore one of the two conditions above MUST be met, and all lots less than 22' in width shall be eligible for this reduced parking requirement.*





DESIGN CODE  
THOROUGHFARE PLAN

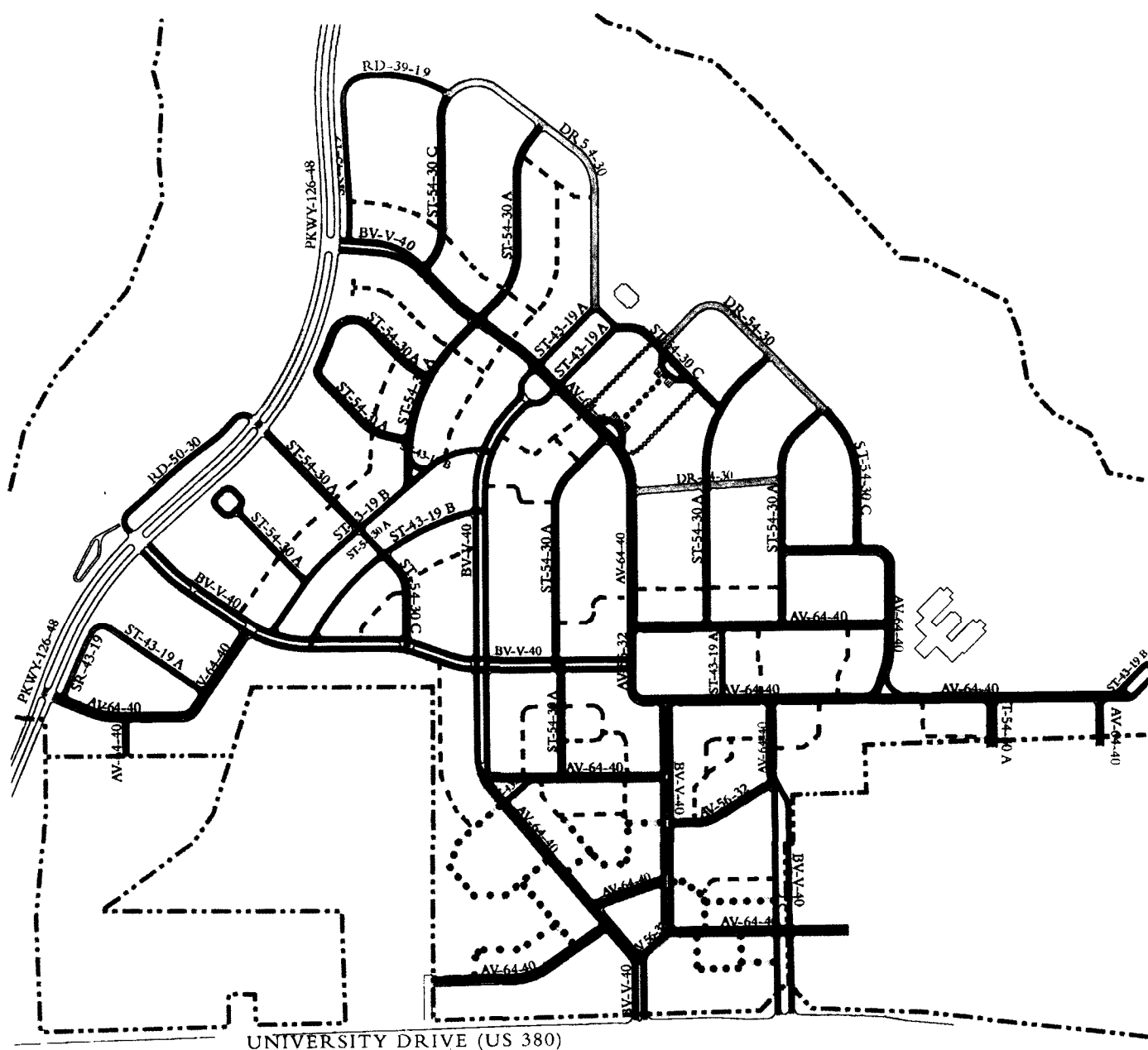
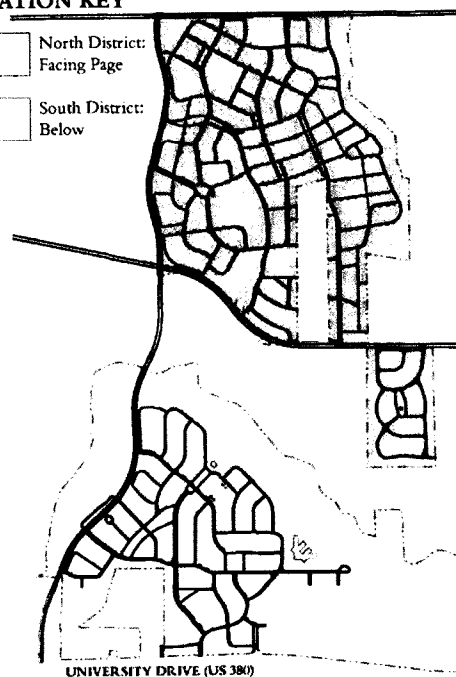


### LOCATION KEY

- ☐ North District:  
Facing Page

☐ South District:  
Below

**NOTE:** All thoroughfares within the Southern Land tract are subject to final approval by the City of McKinney Engineer after review of the Traffic Impact Analysis (TIA).





DESIGN CODE  
THOROUGHFARE SUMMARY

Thoroughfares are a critical piece of any community plan. Not only do they provide the framework for public and private spaces in each individual block; they themselves are public open spaces, essential for movement within the community by foot and vehicle. Each thoroughfare is defined by its capacity and character. Capacity is defined as the number of vehicles that can move safely through a segment of thoroughfare within a given time period. Capacity is dependent on number of travel lanes and their width, provisions for on-street parking, center line radius, curb radius, and elevation of the pavement. Character is defined as the suitability of the thoroughfare as a setting for pedestrian activities and as a location for a variety of building types. Character is dependent on the frontage types and building functions associated with a given thoroughfare. The buildings and the thoroughfare are directly related to their location within the Transect.

The following chart provides characteristics for each of the twenty one thoroughfare sections. The color code used corresponds to the preceding Thoroughfare Plan. The pages to follow include brief descriptions of each of the nine thoroughfare types from Passage (PS) to Parkway (PKWY) as well as scaled diagrams of each individual section. The final portion of the Thoroughfare Standards are the general guidelines and principles for the vehicular, pedestrian, and parking networks.

Note: Street tree placement is dependent on utility locations. All plans and sections contained in the Thoroughfare Standards depict tree placement for illustrative purposes only. In addition, street trees shall be required to have root barriers within planting strips. Root barriers may be placed around individual trees or next to the curb.

THOROUGHFARE TYPE	RIGHT OF WAY		PAVEMENT	NUMBER OF LANES	DIRECTION	MOVEMENT/ DESIGN SPEED	PARKING (WIDTH)	CURB TYPES	TURNING RADIUS *	PLANTERS (WIDTH)	SIDEWALKS (WIDTH)	LANDSCAPE CHARACTER
ALLEY (AL)	15'	12' (10' min)		One	Two Way	Yield (15 mph)	None	Both Sides Swaled	Alley Apron**	N/A	None	
ALLEY (AL)	30'	18'		One	Two Way	Slow (15 mph)	None	Both Sides Swaled	Alley Apron**	N/A	None	N/A
ALLEY (AL)	30'	24'		Two	Two Way	Slow (15 mph)	None	Both Sides Swaled	Alley Apron**	N/A	None	
	43'	19'		One	One Way	Slow (25 mph)	One Side (8')	One Raised/ One Swaled	20' min. 30' max.	One Side (7')	One Side (5')	Natural side has character of Road (RD) or is designed as part of adjacent open space. Urban side has character of Street (ST) or Avenue (AV)
	54'	30'		Two	Two Way	Slow (25 mph)	One Side (8')	One Raised/ One Swaled	20' min. 30' max.	One Side (7')	One Side (5')	
	39'	19'		One	One Way	Slow (25 mph)	One Side (8')	Both Sides Raised	20' min. 30' max.	None	None	Multiple species Naturalistic, irregular clusters Compatible with yard trees
	50'	30'		Two	Two Way	Yield/Slow (25 mph)	Allowed, not designated	Both Sides Raised	20' min. 30' max.	None	None	
	43'	19'		One	One Way	Slow (25 mph)	One Side (8')	Both Sides Raised	20' min. 30' max.	Both Sides (7')	Both Sides (5')	
	43'	19'		One	One Way	Slow (25 mph)	One Side (8')	Both Sides Raised	20' min. 30' max.	One Side (7')	One Side (5')	
	54'	30'		Two	Two Way	Yield/Slow (25 mph)	Allowed, not designated	Both Sides Raised	20' min. 30' max.	Both Sides (7')	Both Sides (5')	Limited (one or two) species Continuous planters Regularly spaced allees or staggered rows Compatible with yard trees
	54'	30'		Two	Two Way	Yield/Slow (25 mph)	Allowed, not designated	Both Sides Raised	20' min. 30' max.	One Side (7')	One Side (5')	
	54'	30'		Two	Two Way	Free (25 mph)	One Side (8')	Both Sides Raised	20' min. 30' max.	Both Sides (7')	Both Sides (5')	
	54'	30'		Two	Two Way	Free (25 mph)	One Side (8')	Both Sides Raised	20' min. 30' max.	One Side (7')	One Side (5')	
	54'	30'		Two	One Way	Free (30 mph)	One Side (8')	Both Sides Raised	20' min. 30' max.	Both Sides (7')	Both Sides (5')	Single species Continuous planters (less urban areas) or individual tree wells (more urban areas) Regularly spaced allees Compatible with yard trees
	56'	32'		Two	Two Way	Free (30 mph)	One Side (8')	Both Sides Raised	20' min. 30' max.	Both Sides (7')	Both Sides (5')	
	64'	40'		Two	Two Way	Free (30 mph)	Both Sides (8')	Both Sides Raised	20' min. 30' max.	Both Sides (7')	Both Sides (5')	
	82' min.	40'		Two (Divided)	Two Way	Free (35 mph)	Both Sides (8')	Both Sides Raised	20' min. 30' max.	Both Sides (5')	Both Sides (5')	(Same character as Avenue (AV))
	43'	19'		One	One Way	Slow (25 mph)	One Side (8')	Both Sides Raised	20' min. 30' max.	One Side (7')	One Side (5')	N/A
PARKWAY (PKWY)	126'	48'		Four (Divided)	Two Way	Free (45 mph)	None	Both Sides Raised	25' min. 35' max.	None	None	
PARKWAY (PKWY)	126'	72'		Six (Divided)	Two Way	Free (45 mph)	None	Both Sides Raised	25' min. 35' max.	None	None	
PASSAGE (PS)	16'	6'		One	N/A	Pedestrian	N/A	N/A	N/A	N/A	N/A	N/A

\* The Turning Radius provided for each individual thoroughfare type is intended to be the inside radius of the travel lane, not necessarily the curb radius. Please refer to the Reduced Intersection Radius diagram that follows.

\*\* An Alley Apron is defined as a transitional flare-out that provides the minimum twenty (20) foot turning radius for vehicles entering and exiting a private alley.

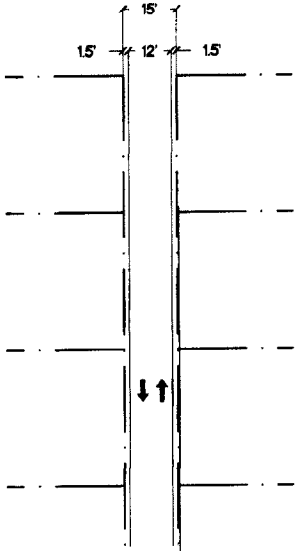
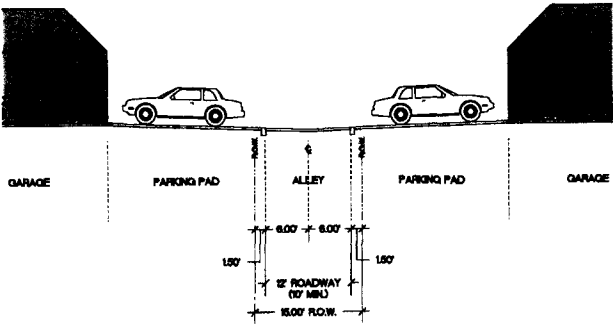




Thoroughfare Type  
Right-Of-Way (R.O.W)  
Pavement Width

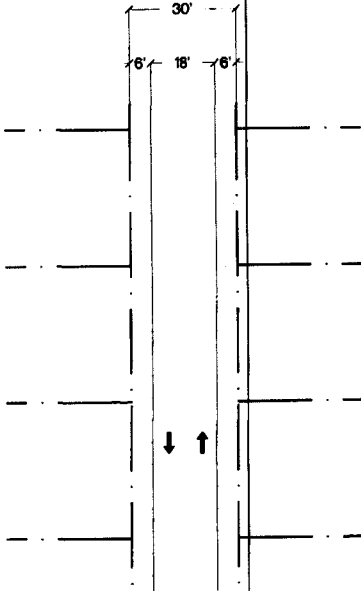
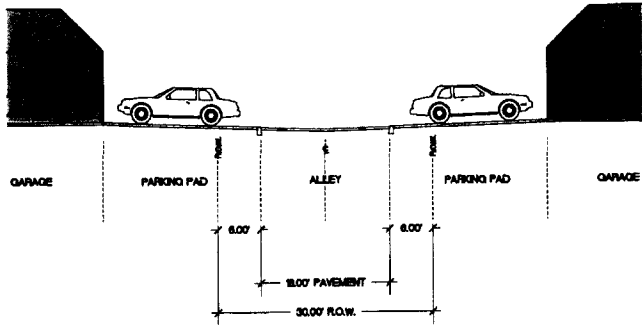
AL ALLEY (AL-15-12)

This vehicular thoroughfare type is located to the rear of more urban lots. Alleys provide vehicular access to service and parking and may contain utility easements. Alleys are paved from building face to building face and do not contain sidewalks. Drainage occurs by inverted crown at the center and roll curbs at the edges. This particular section is intended for use in residential areas and is shown with a twelve (12) foot pavement width. The minimum pavement width for a residential alley is ten (10) feet.



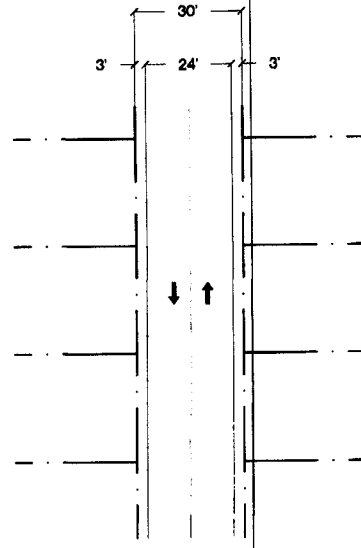
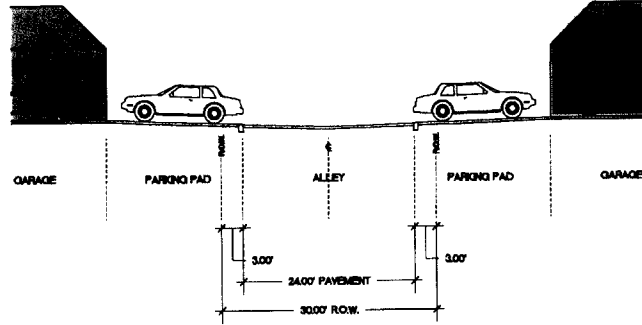
AL ALLEY (AL-30-18)

This vehicular thoroughfare type is located to the rear of more urban lots. Alleys provide vehicular access to service and parking and may contain utility easements. Alleys are paved from building face to building face and do not contain sidewalks. Drainage occurs by inverted crown at the center and roll curbs at the edges. This particular section is intended for use in commercial areas where additional pavement width may be required.



AL ALLEY (AL-30-24)

This vehicular thoroughfare type is located to the rear of more urban lots. Alleys provide vehicular access to service and parking and may contain utility easements. Alleys are paved from building face to building face and do not contain sidewalks. Drainage occurs by inverted crown at the center and roll curbs at the edges. This particular section is intended for use in commercial areas where additional pavement width may be required.

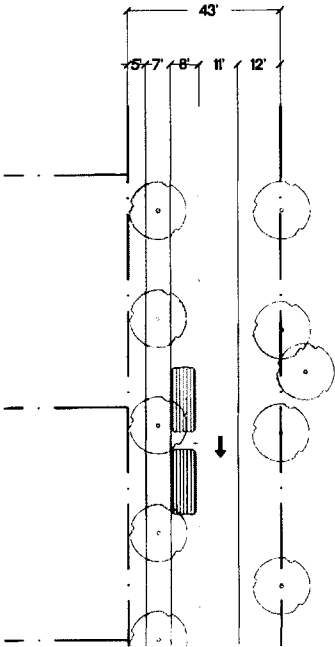
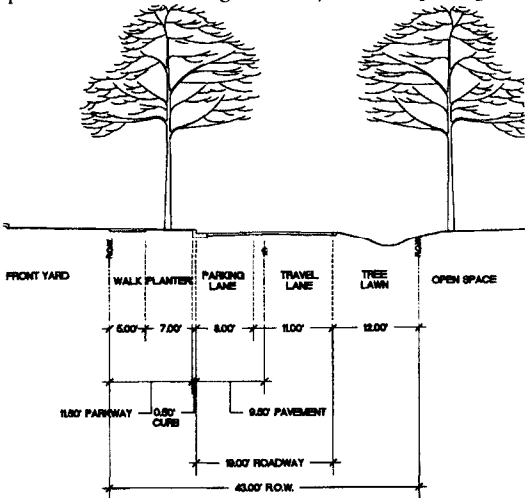




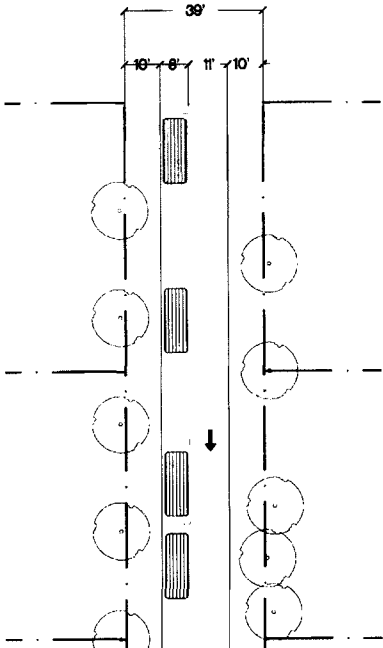
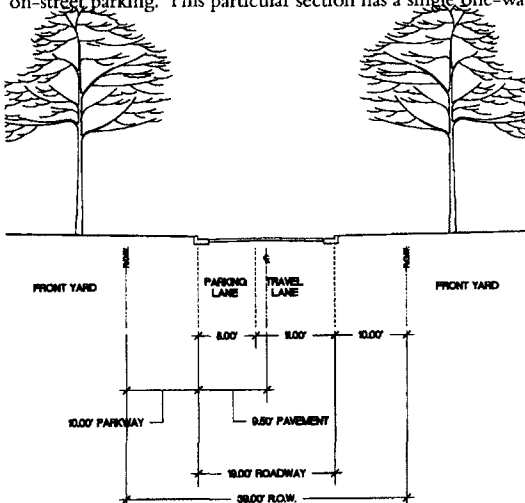
DESIGN CODE  
THOROUGHFARE SECTIONS

DR DRIVE (DR-0-19)

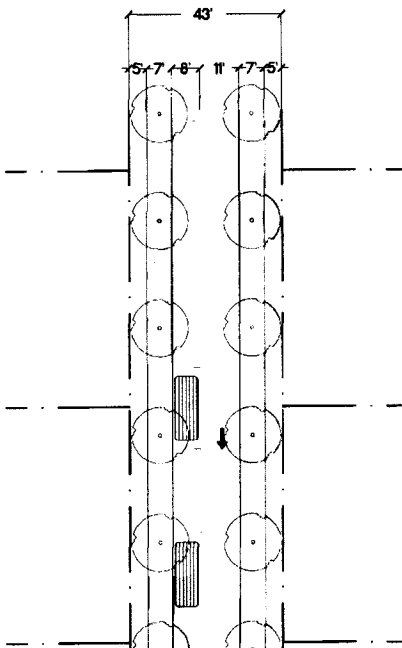
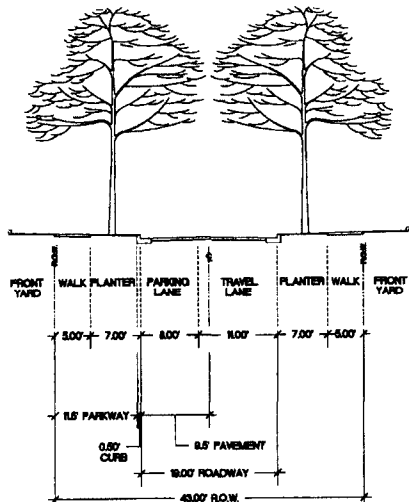
This vehicular and pedestrian thoroughfare type generally occurs as a boundary between a natural condition (waterfront, park, etc) and an urbanized area. The more natural side of a drive has the rural character of a Road (RD) or Parkway (PKWY). The streetscape (planting, walkways, etc) on the side adjacent to the open space is designed as part of that open space. The more urban side has the character of a Street (ST), with a sidewalk and buildings defining the public frontage. This particular section is a single one-way lane with parking on one side.



This vehicular and pedestrian thoroughfare type occurs in the more rural Transect Zones (T3) and is categorized by low speed and low capacity. Roads may have a walking path or bicycle trail along one or both sides. The public frontage consists of raised curbs and drainage occurs by inlets. Landscaping consists of a variety of species arranged in naturalistic clusters. A road from which garages are accessed by front-loaded driveways, thereby providing additional off-street parking, is not required to provide on-street parking. This particular section has a single one-way lane with parking on one side.



This vehicular and pedestrian thoroughfare type occurs in the more urban Transect Zones (T4 and T5). Streets are categorized by low speed and low capacity. The public frontage consists of raised curbs and drainage occurs by inlets. Sidewalks are separated from the vehicular lanes by a planting strip with regularly spaced trees and parking on both sides. This particular section has a single one-way lane with parking on one side.

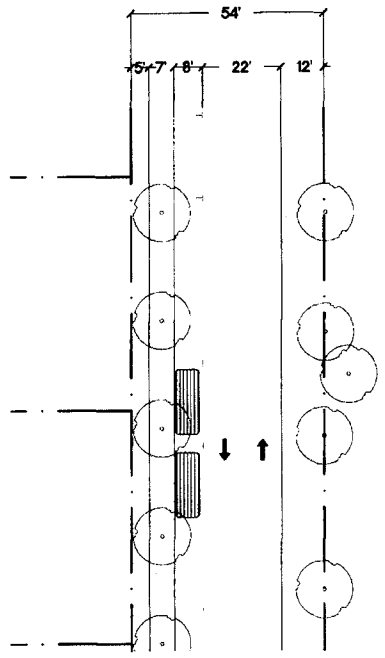
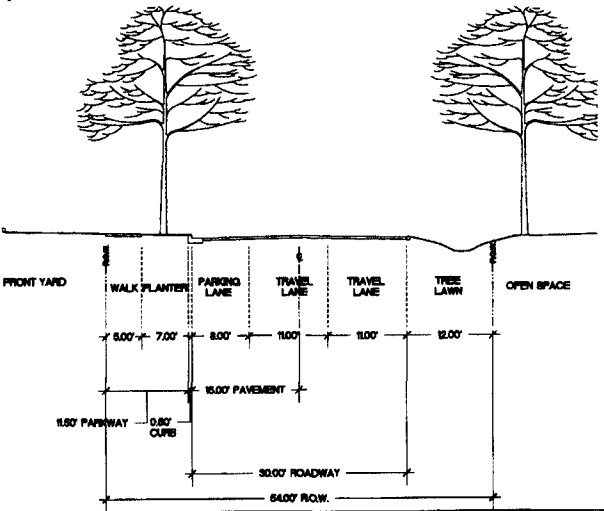




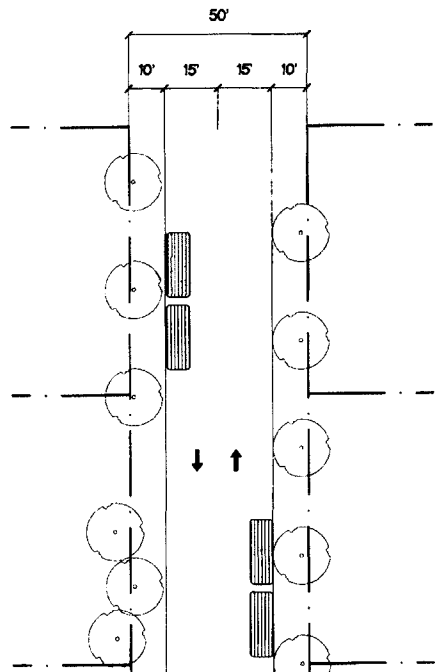
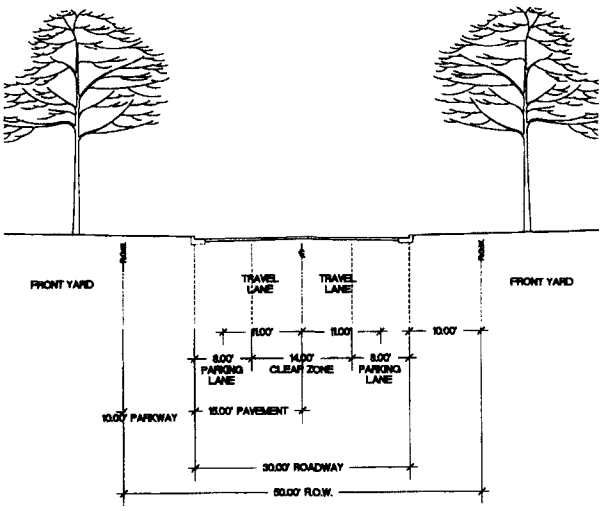
Thoroughfare Type  
Right-Of-Way (R.O.W)  
Pavement Width

**DR DRIVE (DR-54-30)**

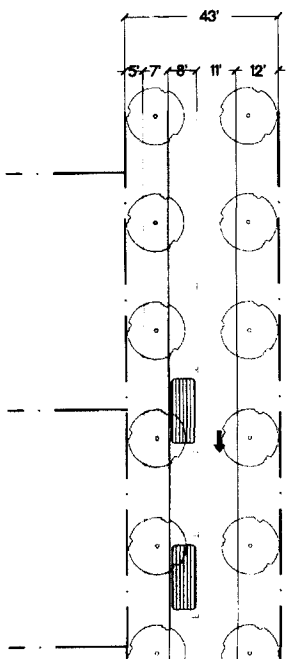
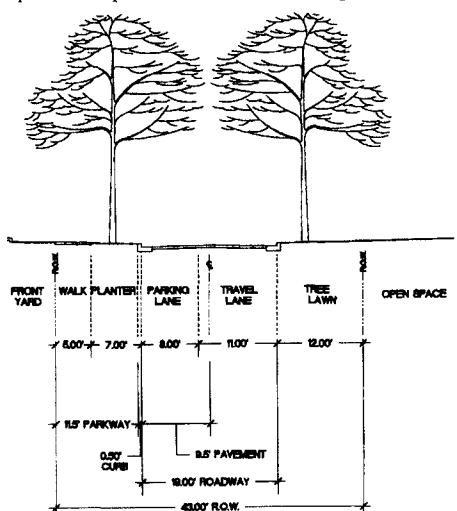
This vehicular and pedestrian thoroughfare type generally occurs as a boundary between a natural condition (waterfront, park, etc) and an urbanized area. The more natural side of a drive has the rural character of a Road (RD) or Parkway (PKWY). The streetscape (planting, walkways, etc) on the side adjacent to the open space is designed as part of that open space. The more urban side has the character of a Street (ST), with a sidewalk and buildings defining the public frontage. This particular section is a pair of two-way lanes with parking on one side.



This vehicular and pedestrian thoroughfare type occurs in the more rural Transect Zones (T3) and is categorized by low speed and low capacity. Roads may have a walking path or bicycle trail along one or both sides. The public frontage consists of raised curbs and drainage occurs by inlets. Landscaping consists of a variety of species arranged in naturalistic clusters. A road from which garages are accessed by front-loaded driveways, thereby providing additional off-street parking, is not required to provide on-street parking. This particular section has a pair of two-way lanes with parking allowed on both sides but not designated.



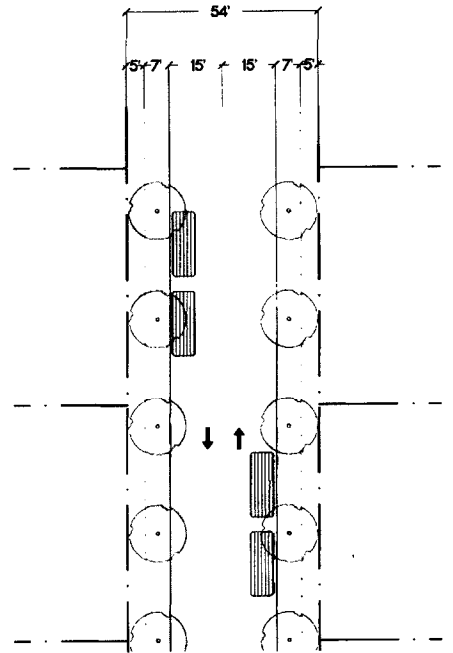
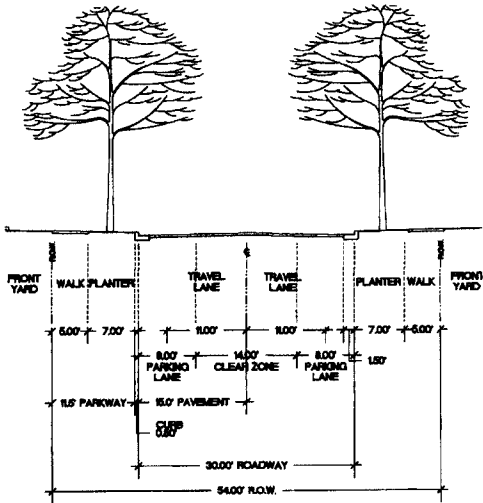
This vehicular and pedestrian thoroughfare type occurs in the more urban Transect Zones (T4 and T5). Streets are categorized by low speed and low capacity. The public frontage consists of raised curbs and drainage occurs by inlets. The sidewalk on one side is separated from vehicular lanes by a planting strip with regularly spaced trees. The opposite side has the character of the adjacent open space. This particular section has a single one-way lane with parking on one side.



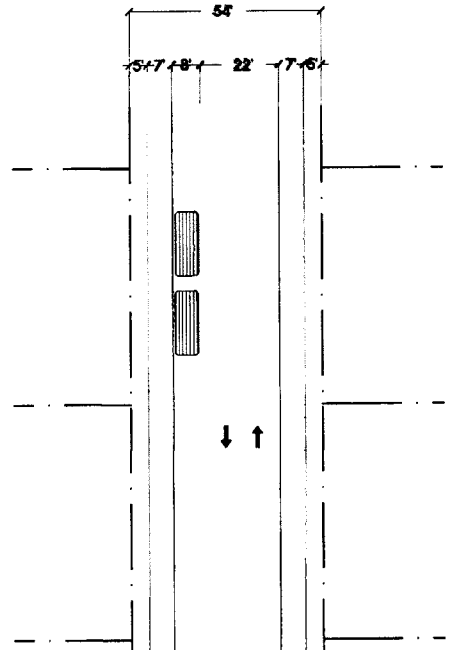
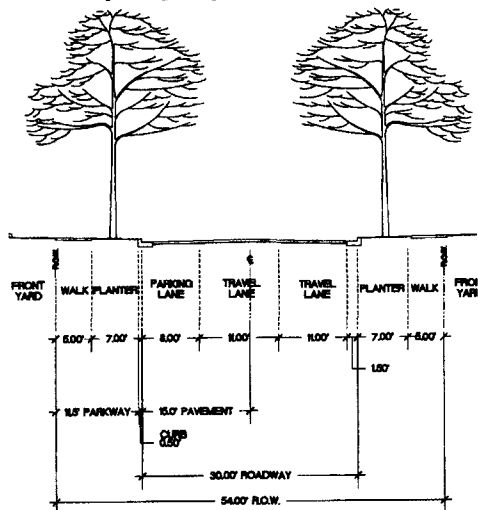


DESIGN CODE  
THOROUGHFARE SECTIONS

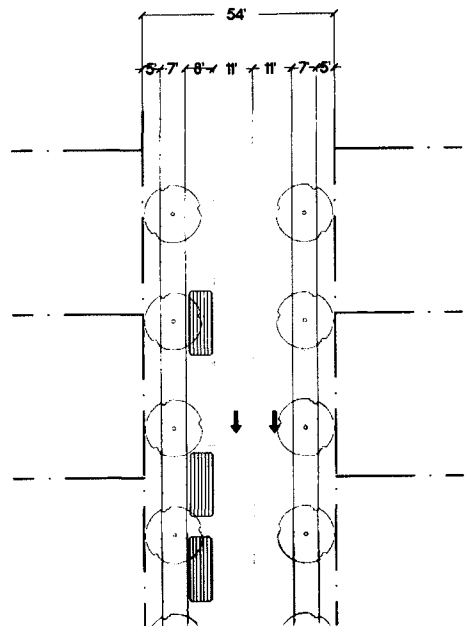
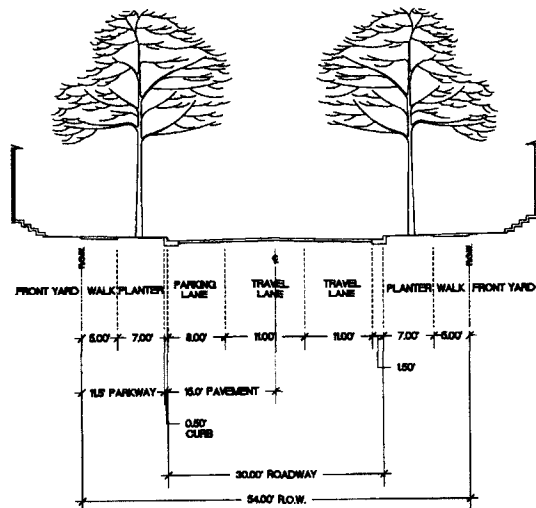
This vehicular and pedestrian thoroughfare type occurs in the more urban Transect Zones (T4 and T5). Streets are categorized by low speed and low capacity. The public frontage consists of raised curbs and drainage occurs by inlets. Sidewalks are separated from vehicular lanes by a planting strip with regularly spaced trees and parking on both sides. This particular section is a pair of two-way lanes with parking allowed but not designated.



This vehicular and pedestrian thoroughfare type occurs in the more urban Transect Zones (T4 and T5). Streets are categorized by low speed and low capacity. The public frontage consists of raised curbs and drainage occurs by inlets. Sidewalks are separated from vehicular lanes by a planting strip with regularly spaced trees and parking on both sides. This particular section is a pair of two-way lanes with parking designated on one side.



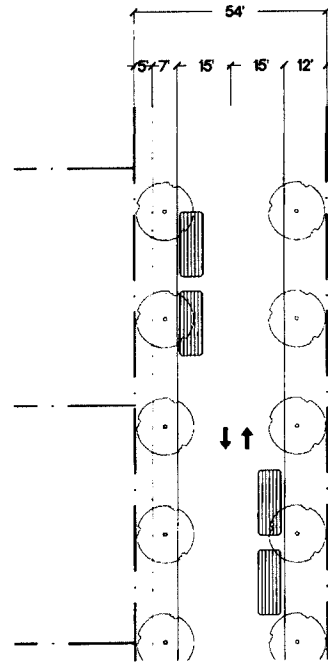
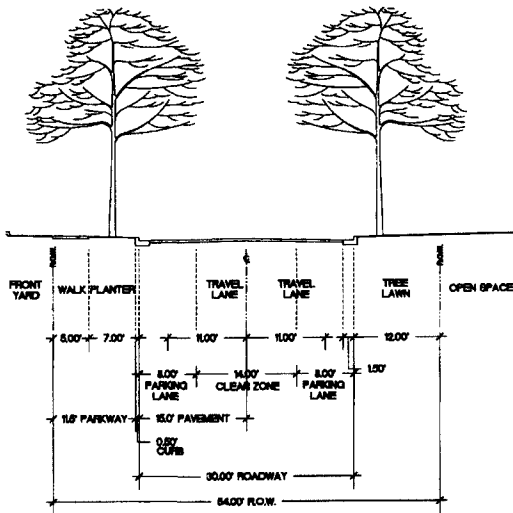
This vehicular and pedestrian thoroughfare type is defined as a short distance connector. Avenues are designed for low speed and high capacity. They are an urban thoroughfare type, categorized by raised curbs and on-street parking on both sides. In more urban transects, trees are placed in individual planters. This particular section has a pair of one-way lanes with parking on one side.



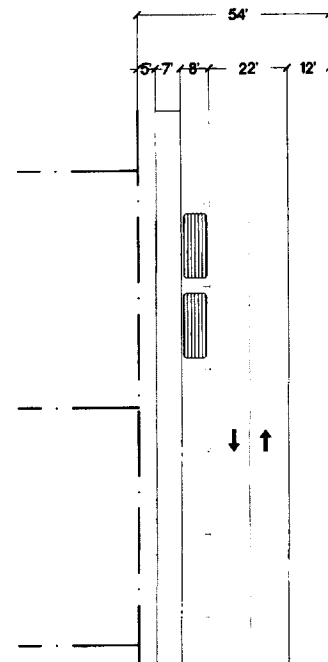
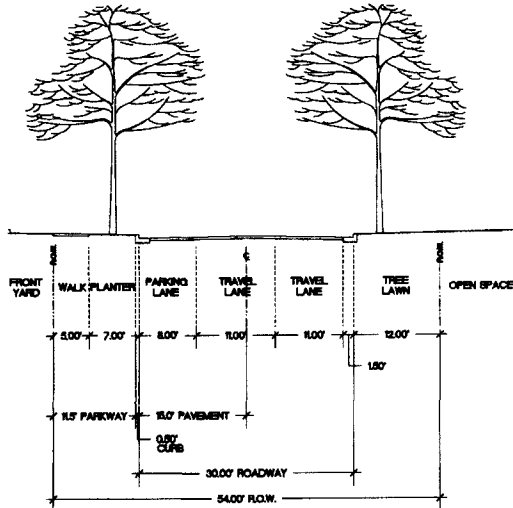


Thoroughfare Type  
Right-Of-Way (R.O.W)  
Pavement Width

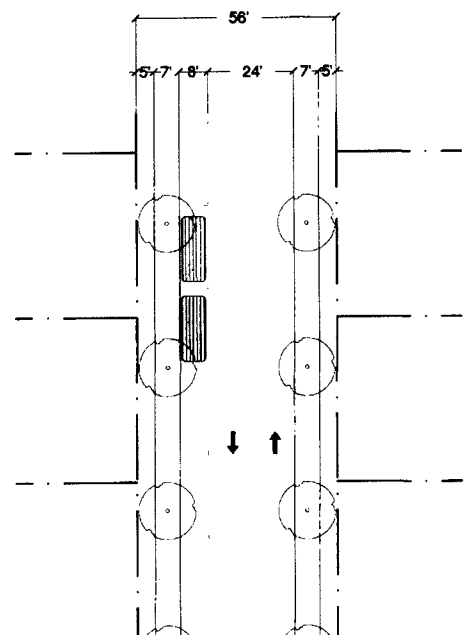
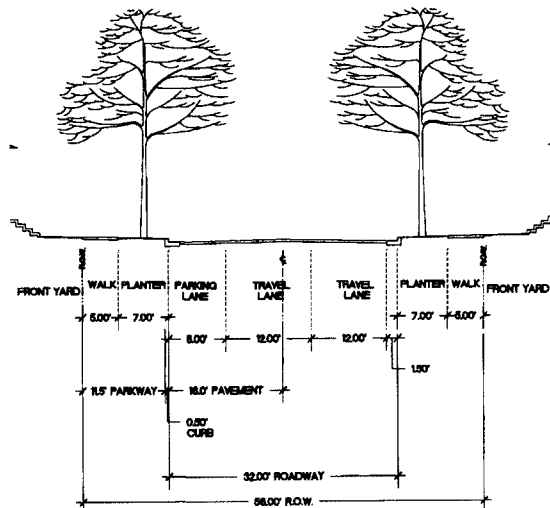
This vehicular and pedestrian thoroughfare type occurs in the more urban Transect Zones (T4 and T5). Streets are categorized by low speed and low capacity. The public frontage consists of raised curbs and drainage occurs by inlets. The sidewalk on one side is separated from vehicular lanes by a planting strip with regularly spaced trees. The opposite side has the character of the adjacent open space. This particular section is a pair of two-way lanes with parking allowed but not designated.



This vehicular and pedestrian thoroughfare type occurs in the more urban Transect Zones (T4 and T5). Streets are categorized by low speed and low capacity. The public frontage consists of raised curbs and drainage occurs by inlets. The sidewalk on one side is separated from vehicular lanes by a planting strip with regularly spaced trees. The opposite side has the character of the adjacent open space. This particular section is a pair of two-way lanes with parking designated on one side.



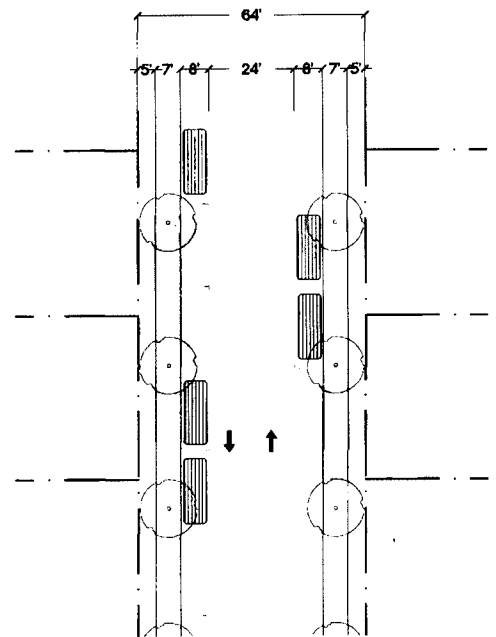
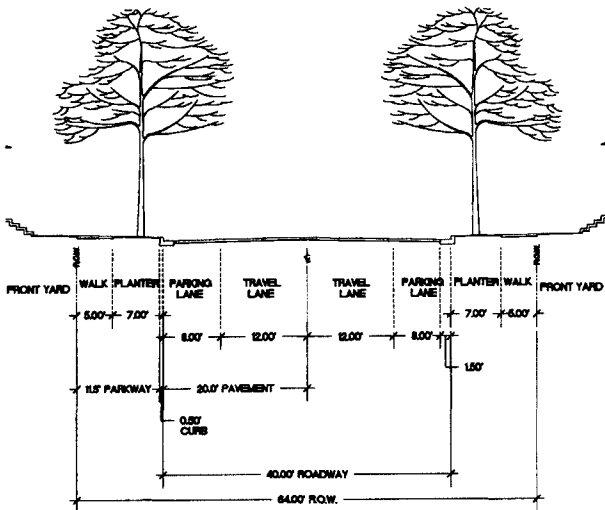
This vehicular and pedestrian thoroughfare type is defined as a short distance connector. Avenues are designed for low speed and high capacity. They are an urban thoroughfare type, categorized by raised curbs and on-street parking on both sides. In more urban transects, trees are places in individual planters. This particular section has a pair of two-way lanes and parking on one side.





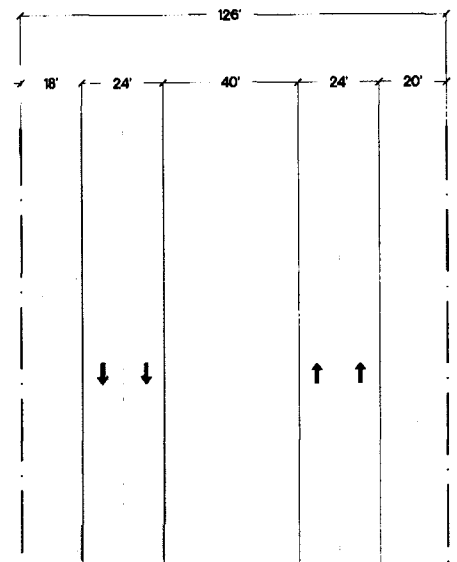
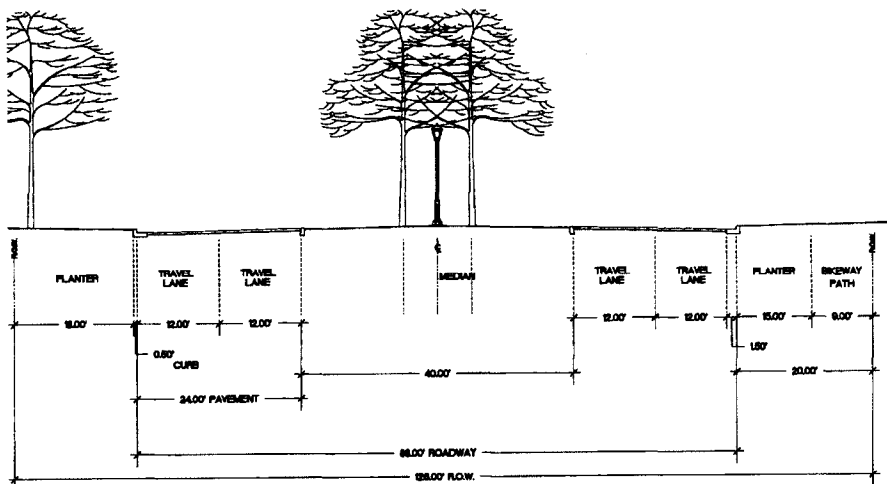
DESIGN CODE  
THOROUGHFARE SECTIONS

This vehicular and pedestrian thoroughfare type is defined as a short distance connector. Avenues are designed for low speed and high capacity. They are an urban thoroughfare type, categorized by raised curbs and on-street parking on both sides. In more urban transects, trees are placed in individual planters. This particular section has a pair of two-way lanes and parking on both sides.

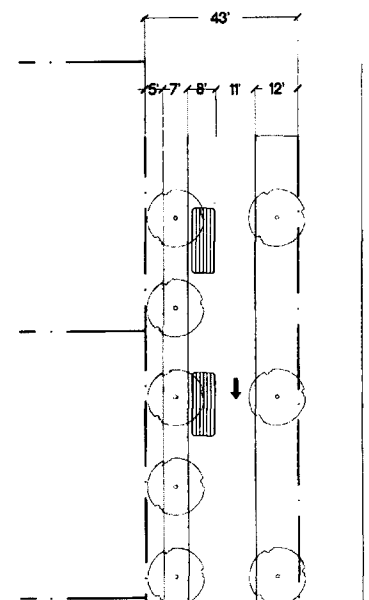
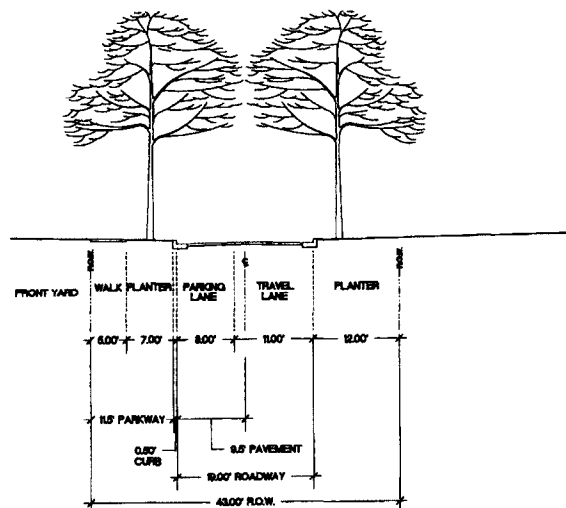


PKWY PARKWAY (PKWY-126-48)

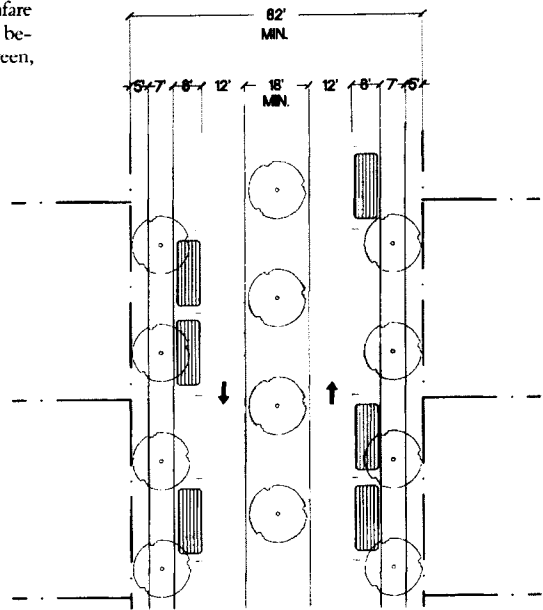
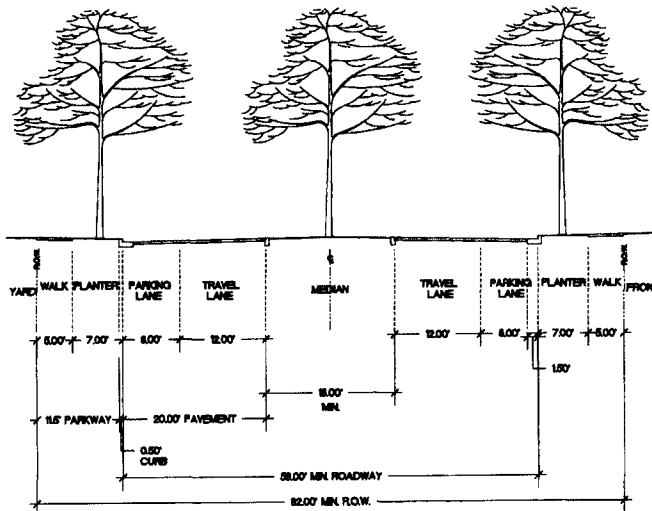
This vehicular and pedestrian thoroughfare type is a long-distance boulevard that connects urban centers. Traffic moves along two-way lanes, often with medians. One-way Slip Roads (SR) are frequently provided running parallel on either side. This particular section has four divided lanes with a 40' median between and no on-street parking. This section allows for future construction of two additional lanes within the 40' median as shown in the section below (PKWY-126-72).



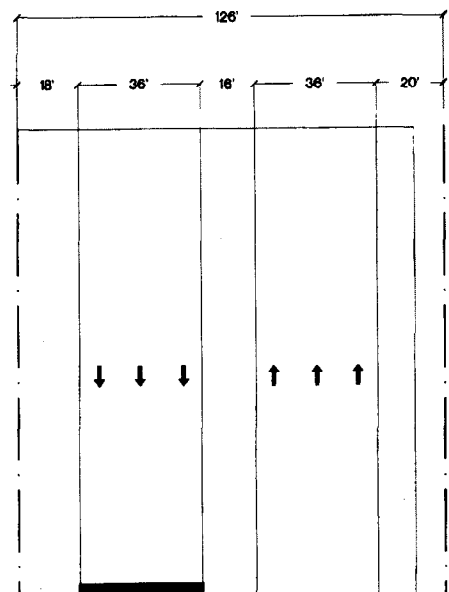
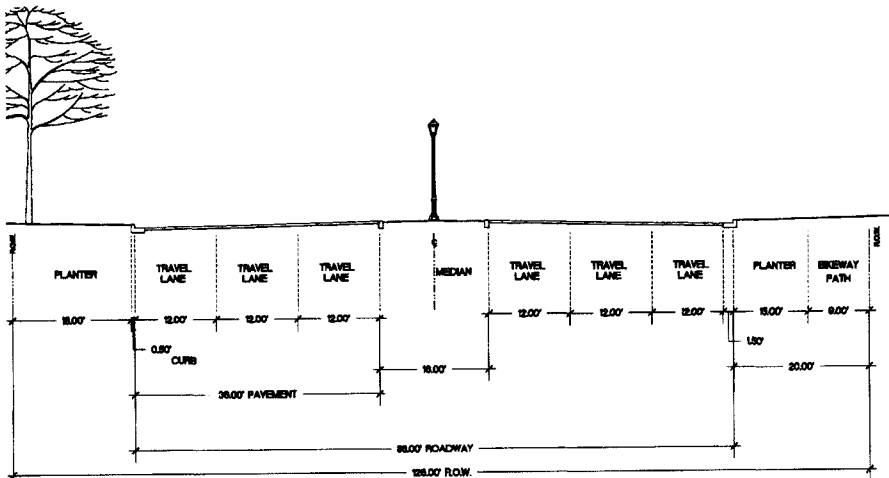
This vehicular and pedestrian thoroughfare type runs parallel to high-capacity, high-speed thoroughfares types such as Parkways (PKWYS). Slip roads are provided as a low-capacity, low-speed alternate route along such a thoroughfare. Movement is one-way with parking on one side.



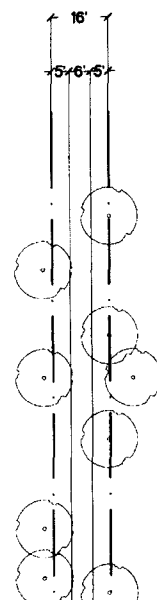
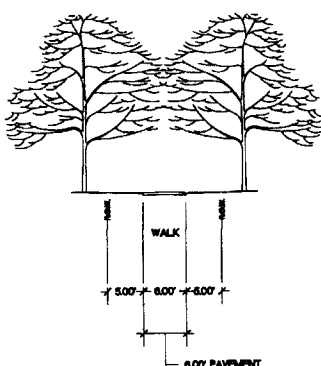
This vehicular and pedestrian thoroughfare type is defined as a short distance connector between neighborhood centers. Boulevards are designed for low speed and high capacity. They are an urban thoroughfare type, categorized by raised curbs and on-street parking on both sides. A landscaped median is provided between one-way lanes. This particular section has two divided lanes with an 18' minimum median between, however this width (and therefore the R.O.W. width) is variable depending on landscape conditions.



This vehicular and pedestrian thoroughfare type is a long-distance boulevard that connects urban centers. Traffic moves along two-way lanes, often with medians. One-way Slip Roads (SR) are frequently provided running parallel on either side. This section is an improvement upon the section above (PKWY-126-48) with six divided lanes. The addition of two lanes within the previous 40' median leaves a 16' median which can be eliminated where a left turn lane is necessary. There is no on-street parking.



This pedestrian thoroughfare type serves as a walkable connector between buildings. Passages provide shortcuts through long blocks and connect rear parking areas to frontages. Passages are permitted to be roofed over. This thoroughfare type is acceptable in Transect Zones T3, T4, and T5.



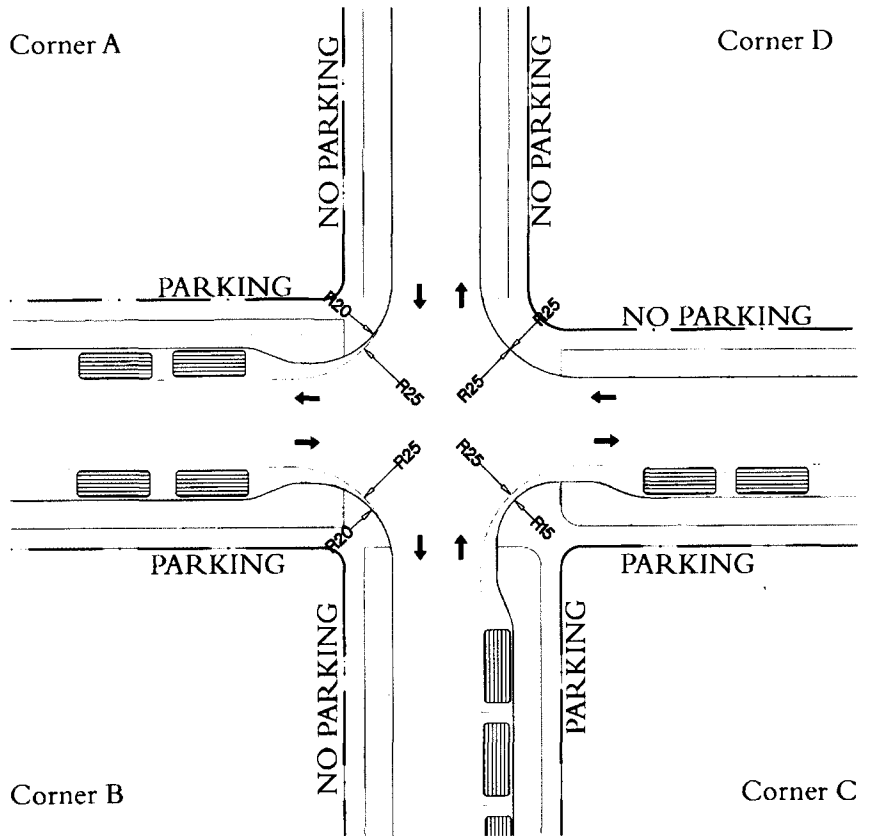


### REDUCED INTERSECTION RADIUS

A Reduced Intersection Radius is a method of intersection design and traffic-calming used on streets with on-street parking. As the illustration depicts, the first parking space at each intersection is moved at least twenty (20) feet back from the corner. A "no parking" zone is created between the first parking space and the intersection by way of signage and/or pavement striping. This configuration maintains a twenty (20) foot curb radius while providing a twenty five (25) foot vehicular turning radius. This radius is more than ample for emergency vehicles and large trucks to navigate. As an added precaution, the single handicap ramps at each corner are designed as mountable curbs. This type of ramp allows pedestrians and the handicapped to follow the sidewalk perpendicularly, straight to the crosswalk, rather than into the middle of the intersection.

The illustration at right shows four different types of right-turn conditions. Corner A illustrates a turn from a street without on-street parking to a street with on-street parking. Corner B illustrates a turn from a street with on-street parking to a street without on-street parking. Corner C illustrates a turn from a street with on-street parking to a street with on-street parking. Corner D illustrates a turn from a street without on-street parking to another street without on-street parking. This fourth intersection type is unable to employ the Reduced Intersection Radius method, and therefore the curb must be designed to minimum standards.

The diagram illustrates the standard intersection criteria that will be implemented throughout the Southern Land community. However, for each individual phase of the development, a full set of large-scale intersection diagrams will be provided for City of McKinney Fire and Engineering department approval.



### THOROUGHFARES WITH PEDESTRIAN FRONTAGE

Pedestrian thoroughfare frontages are conditions in which the primary frontage of a lot is adjacent to a pedestrian right of way (as opposed to a vehicular thoroughfare). The two most frequently found variants of this conditions are the Bungalow Court (Figure C) and the Pedestrian Mews (Figure D). In both scenarios, emergency vehicle access is provided directly in the pedestrian right of way with the use of a structural grass paver system.

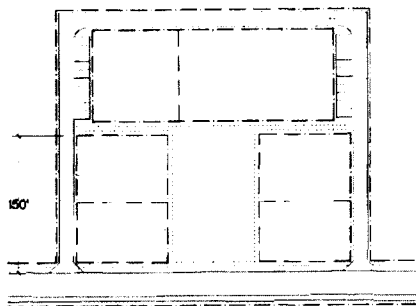


Figure C

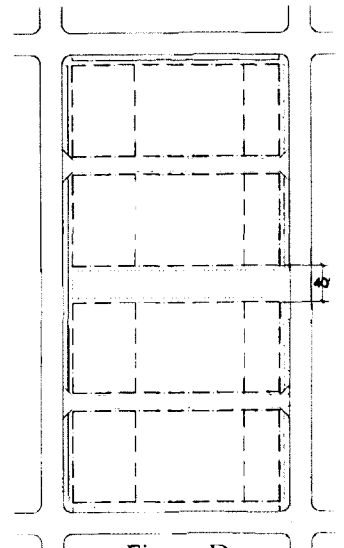


Figure D



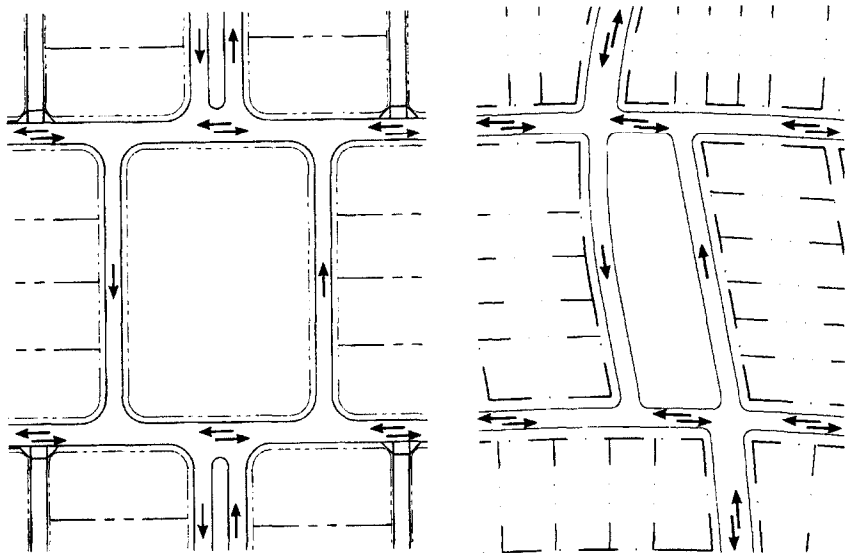


Figure E

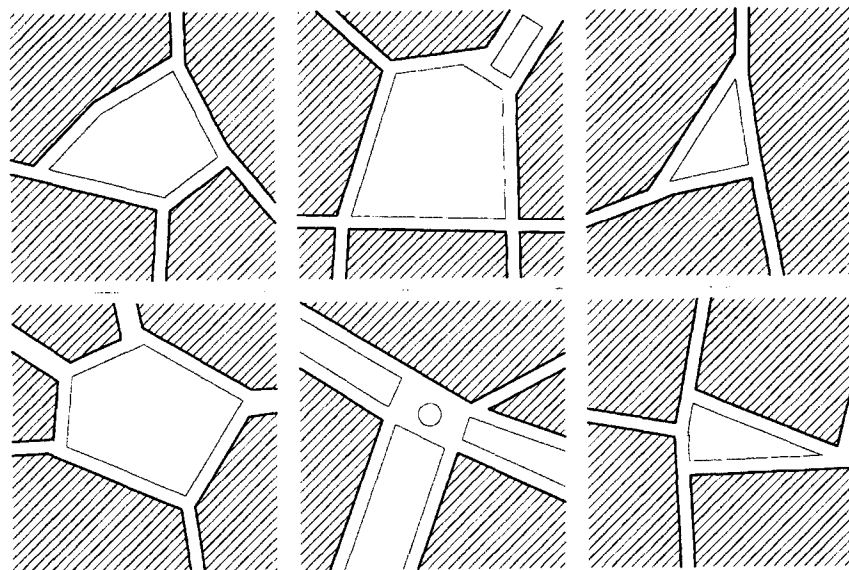
Figure F

**INTERSECTION OFFSET**

Intersection Offset is another traffic calming device used where appropriate. The diagram at far left depicts a carefully designed offset intersection as-built in an existing Southern Land community. The diagram at left is proposed in the north portion of the Southern Land tract in McKinney.

The offset intersection is used to reduce the number of collision points as well as to increase the level of driver awareness due to the unique configuration. This results in a slower, safer intersection with fewer accidents.

(Diagrams are at 1"=150' scale)

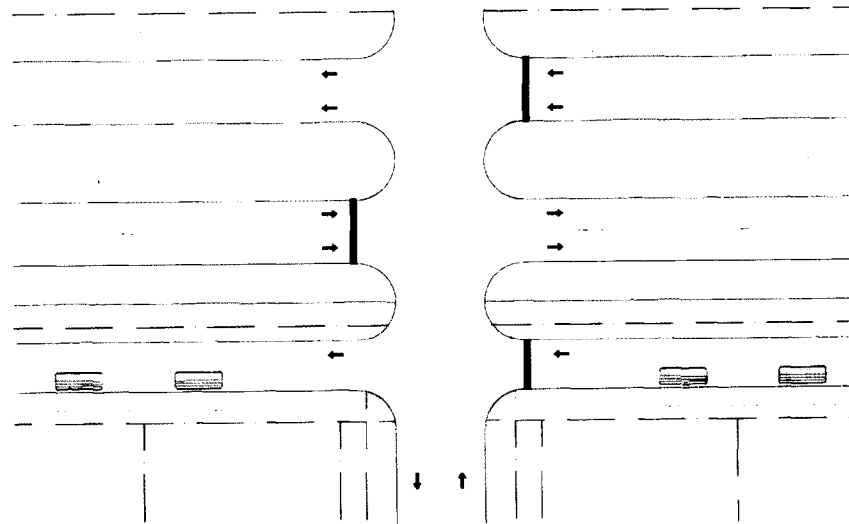


**INTERSECTION DEFLECTION**

Intersection Deflection is the angle at which one thoroughfare intersects another. A standard intersection is one at which the thoroughfares intersect at, or close to, ninety degrees. Frequently, the topography or the urban plan creates an instance where it is necessary and beneficial to create intersections that have an extreme angle of deflection.

The result of these deflected intersections is a safer, more pedestrian friendly neighborhood plan. These unusual conditions cause drivers to be more cautious and alert at the intersection. Like the intersection offset, a deflected intersection serves as a traffic calming device and often eliminates the need for less desirable speed-reducing techniques such as speed bumps.

The diagrams at left represent some of the proposed intersections at neighborhood centers within the Southern Land community. Specific intersections can be found within the Illustrative Master Plan.



**TYPICAL SLIP ROAD/PARKWAY  
VEHICULAR CIRCULATION**

The diagram at left illustrates movement of vehicles on a parkway (PKWY) and a parallel slip road (SR). Typically, traffic on the one-way slip road moves in the same direction as the parkway lanes farthest from the slip road. Therefore, traffic on the slip road and the adjacent parking lanes move in opposite directions.



## DESIGN CODE

### THOROUGHFARE STANDARDS

#### THOROUGHFARE NETWORK STANDARDS

The thoroughfare network in Somerset Waters has been developed on the following basic principles:

- \* Interconnectivity throughout the development and adjacent spaces is provided for vehicles, bicycles, and pedestrians.
- \* Within the neighborhood centers, thoroughfares converge at common areas, such as commercial buildings, civic spaces, transit centers, or open spaces.
- \* Thoroughfare network accommodates the automobile without compromising pedestrian movement.
- \* Thoroughfare network provides adequate and efficient servicing of the development by trucks and utility vehicles. The visual and noise impact of such services is minimized through the use of alleys to keep operations behind the building and not in the public right-of-way.
- \* All thoroughfares terminate at either end at intersections with other streets. Cul-de-sacs are strongly discouraged. Where cul-de-sacs are necessary because of topography or environment, a pedestrian/bicycle path is provided to the adjacent street.
- \* Bicyclists and pedestrians are able to travel on local streets to most locations without the need to follow arterials.
- \* Pedestrian crosswalks are provided at all intersections throughout the development.
- \* Townhomes or detached residences do not directly front Highway 380. They may be oriented toward an arterial parkway, provided that they front a slip road.
- \* Curb type used throughout will be to City of McKinney standards.

#### DRIVEWAY STANDARDS

- \* The minimum width of a residential driveway shall be eleven (11) feet.
- \* The maximum width of a residential driveway shall be twenty four (24) feet.
- \* Driveways shared by adjacent lots shall have no less than nine (9) feet of pavement on each property (18' minimum width).
- \* Curb radii for a residential drive shall be no less than five (5) feet and no greater than ten (10) feet.
- \* Circular drives are not permitted on streets with parking on only one side.
- \* Private driveways providing the only means of access to one or more lots shall be designed to City of McKinney Fire Department standards.

#### PEDESTRIAN NETWORK STANDARDS

- \* Sidewalks shall be provided whenever possible within the neighborhood centers. Sidewalks should be provided on at least one side of the street in T4 zones, and on both sides in T5 zones.
- \* Within the Mixed Use Urban Centers, sidewalks are required on all public streets with building frontage. Throughout the development, sidewalks are required on any thoroughfare with non-residential uses on the ground floor. These sidewalks shall be no less than eight (8) feet wide with a minimum of five (5) feet of unobstructed walkways. Ten (10) foot wide sidewalks are strongly recommended.
- \* On residential streets, sidewalks shall be no less than four (4) feet wide. Five (5) foot sidewalks are strongly recommended, and are the standard width for the proposed thoroughfare sections.
- \* Sidewalks shall not be obstructed by telephone poles or other barriers. Fire hydrants are not included in this requirement, however a minimum of four (4) feet of width shall remain clear in residential areas and five (5) feet of width shall remain clear in Mixed-Use Urban Centers.
- \* Sidewalks should be protected by shade trees where possible. In residential areas, this should be achieved through the use of a planting strip between the road and the sidewalk no less than five (5) feet wide. The trees planted in this strip, at maturity, should provide a continuous canopy along the sidewalk. In non-residential areas, the planting strip may be minimized to tree wells containing

ornamental scale trees.

- \* All entrances and exits to public and commercial and/or mixed-use buildings shall be connected to the public sidewalk by a continuous pedestrian walkway.

#### PARKING NETWORK STANDARDS

##### Off-Street Parking:

- \* Off-street parking shall be provided for all uses in T3, T4, and T5.
- \* Off-street parking shall be located on the side or rear of the building, or underground or in a parking structure, but not between the building and the pedestrian route.
- \* At least 80% of the surface parking spaces for commercial buildings should be located behind or alongside the building. (20% on-street)
- \* Surface parking lots may front parkways and highways but shall not be located between a building and any other thoroughfare type.
- \* If surface parking is located next to the building, the surface parking area shall be screened by a fence, wall, or hedge in order to continue the build-to-line of the adjacent building facades.
- \* If adjacent uses complement one another in terms of the days and time of uses, shared parking shall be allowed per the standards set forth in City of McKinney Zoning Ordinance (Sec. 41-202) and as subject to the approval of the Director of Planning. Approval shall be based on walking distance from parking to destination.
- \* Parking lots should be laid out to continue the street/block pattern of the area so that lots can easily be redeveloped with buildings consistent with the urban design of the surrounding development.
- \* Surface parking lots in view of a public street with more than fifty (50) spaces are discouraged. Parking lots in public view should be screened and broken up visually with landscaped islands. Lots at the rear of buildings, out of public view, are required to have end islands, but not other landscaped islands.
- \* Surface parking lots located anywhere throughout the development with more than 100 spaces are strongly discouraged.
- \* Surface parking lots serving neighborhood centers should be no larger than 25 spaces per lot. Parking lots larger than 25 spaces should be segregated into multiple, interconnected landscaped lots.
- \* Lots serving adjacent businesses should be connected.
- \* Surface parking lots in view of a public street should utilize a perimeter screening method such as low dense plantings, low walls, trees, retaining walls to lower the level of the lot, or a combination of the above.
- \* Pedestrians should have accessibility between the public streets and the building without having to walk through a surface parking lot, unless the parking lot is transected with a covered walkway of at least fifteen feet, connecting the building to the street.
- \* Direct, defined and protected pedestrian routes should be provided through parking lots.

##### On-street Parking

- \* On-street parking should be provided wherever possible on thoroughfares except highways and arterials.
- \* On-street parking may count towards parking requirements when located within 500' of the usage or per the approval of the planning director.

##### Parking Structures

- \* Structured parking facilities with frontage on primary streets shall be designed so that the street level frontage is retail, commercial, or civic space or the parking structure is designed with an architecturally articulated facade to screen the parking areas of the structure.

##### Bicycle Parking

- \* Bicycle parking shall be provided for employees and customers adjacent to the front doors of the building wherever possible.
- \* Facilities to lock bicycles shall be provided for each building with commercial uses greater than 5,000 square feet. Generally, for every twenty motorized vehicle parking spaces, one (1) to two (2) bicycle spaces should be provided (5-10% of vehicular parking requirement).

##### Shared Parking

- \* A church may share commercial parking spaces that are within 600 feet of the church's property.
- \* Residential dwellings may share office use parking spaces that are within 300 feet of the resident's building.
- \* Lodging rooms may share office use parking spaces that are within 600 feet of the lodging room.
- \* Restaurants may share office use parking spaces that are within 300 feet of the restaurant.
- \* Commercial uses may share parking spaces with other commercial uses that are within 600 feet of each other to the extent that their peak business hours do not coincide.
- \* Other shared parking as allowable under City of McKinney ordinances.

##### Parking Placement

- \* The number of parking spaces required shall be determined by the City of McKinney Zoning Ordinance (Sec. 41-202) and is subject to the approval of the Director of Planning. Both on-street and off-street parking shall count towards the fulfillment of the parking requirement.
- \* The required off-street parking shall be placed a minimum of twenty (20) feet behind the facade of the outbuilding.



Public open spaces within Somerset Waters include, but are not limited to the following types:

**Pedestrian Way:** A street-level exterior public pedestrian way through the middle of a block, at least ten (10) feet wide, providing a connection to a public courtyard or a parallel street.

- \* A pedestrian way should only be located where size and scale of development are large enough to allow interalized servicing and ensure adequate pedestrian flows without detracting from activity on adjacent streets.
- \* The scale of the enclosing walls should provide adequate sunlight and sky view.
- \* Building edges should accommodate public uses, such as shops and restaurants that are accessible from the pedestrian way to encourage activity.
- \* Public seating should be integrated where possible.

**Courtyard:** A landscaped open space in the center of a block with no street frontage.

- \* A courtyard should be large enough to allow for public activities and sunlight during midday. The scale of the enclosing walls should provide adequate sunlight and sky view.
- \* Although inwardly oriented, the courtyard should be considered part of a larger set of connected public spaces with access through an adjacent building or public place such as a pedestrian way, plaza, or galleria.
- \* Courtyards are primarily enclosed by buildings, and should have a higher degree of transparency both at grade and above, to provide overlook and to encourage surveillance of the internal space.
- \* The location and design of entrances to the courtyard are critical in order to ensure that they are clearly identifiable and to promote public use.

**Urban Garden:** An intimate-scale garden in an urban setting.

- \* An urban garden should be located and oriented to provide maximum sunlight during midday.
- \* An urban garden should be enclosed by buildings on two or three sides and open to the street on one or two sides.
- \* Street edges should be screened with a combination of architectural and landscape elements.
- \* Adjoining buildings should contain windows overlooking the garden, but the garden should not provide primary access to an individual building.
- \* The design of an urban garden should not create visual obstructions, dead end spaces, or other security hazards.
- \* Each garden "room" should contain a high-quality focal point such as a water feature or public art.

**Plaza:** A predominantly hard-surfaced space that is a portion of a block with a minimum of one side open to the street.

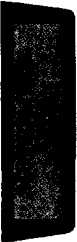
- \* A plaza should be located to provide maximum sunlight during the day, but generally not located at the corner of a block.
- \* A plaza should be defined by the adjacent buildings.
- \* Building edges should contain continuous public uses at grade to animate and support the open space.
- \* Large plazas should provide substantially continuous weather protection through the use of canopies or arcades at their edges.
- \* Unlike intimately scaled urban gardens, a plaza may contain a primary entrance to an adjacent building.
- \* The floor of the plaza should be at the same grade as the sidewalk serving the adjacent street for easy access, with no more than 50% of the square footage deviating no more than three (3) feet above or below grade.

Each of the above open spaces shall provide:

- \* At least one (1) tree or planter
- \* At least one linear foot of seating for each seventy five (75) square feet of open space
  - \* Ledges and benches providing seating on both sides that are at least thirty (30) inches deep shall count double.
  - \* Level and flat planter rims that are not higher than two (2) feet and are at least eight (8) inches deep shall count as seating as long as protruding vegetation does not obstruct them.
  - \* Movable chairs are counted as thirty (30) inches of linear seating per chair and may be stored after hours of operation.
  - \* Corners and the space of steps that are wider than 15 feet may count as seating.
- \* Prior to the issuance of a certificate of occupancy, a plaque shall be placed in a publicly conspicuous location outside the building at street level, or at the side of an outdoor space, identifying the open space furniture, stating the right of the public to use the open space, and the hours of use, and stating the name and address of the owner or owner's agent responsible for maintenance, including the city if the open space is to be maintained by the city.

## STREET LIGHTING STANDARDS

Street lighting standards shall be per City of McKinney Subdivision Ordinance.





## DESIGN CODE

### URBAN CENTER ARCHITECTURAL CODE

This Architectural Code portion of the Design Code is conceived and administered to guide the building of non-single family detached buildings within the neighborhood and mixed use centers. Each center is intended to provide a place of gathering for the residents within the Southern Land Community and the adjacent communities. The relaxed village center character envisioned will be achieved through building placement, architectural design, detail, signage and streetscape/landscape treatments. Buildings will be designed to promote the sense of place that is reminiscent of the historic core of many small Texas towns, including downtown McKinney. The architecture of the buildings and the landscape design of each site will be heavily influenced by the timeless principles of proper scale and proportion and the use of exterior materials that are ageless in appeal and character.

The Architectural Code is a series of prescriptions, some of which are mandatory and some of which are only recommended. The mandatory prescriptions are indicated by the verb shall. The recommended ones are indicated by the verb should. Options that are allowed but neither recommended or discouraged are indicated by the verb may. This applies to the text portion of the Architectural Code only; the images are included to provide general ideas about acceptable styles, materials and details. Meeting the criteria set forth in the Architectural Code does not guarantee DRC approval. Any building in the community, and all of the elements that comprise it, are subject to Design Review Committee approval as well as City approval.

#### ARCHITECTURAL STYLES

##### *Acceptable Architectural Styles*

A substantial portion of the architectural vernacular of North Texas is comprised of the following common pre-World War II architectural styles. These styles are considered appropriate and acceptable for the community of Somerset Waters:

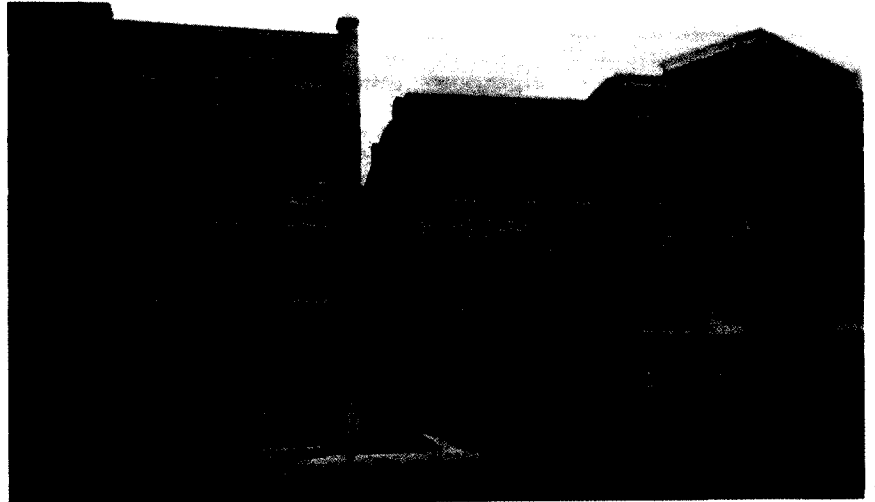
- \* Classical (Federal, Adams, Greek Revival)
- \* Georgian
- \* Colonial Revival
- \* Victorian Vernacular (Second Empire, Shingle style, Richardsonian, and Folk Victorian)
- \* Craftsman (Bungalow)
- \* Tudor (English Tudor, French Eclectic, and English Farmhouse)
- \* Romanesque
- \* Art Deco

##### *Unacceptable Architectural Styles*

The following architectural styles do not compliment the vision for the community of Somerset Waters, and are therefore unacceptable:

- \* Adobe
- \* Contemporary
- \* International Style
- \* New England Salt Box
- \* Oriental
- \* Modern Style with multiple roof lines
- \* Exposed log
- \* Spanish Eclectic style
- \* Coastal
- \* Mission

#### ARCHITECTURAL STYLES, MASSING AND COMPOSITION



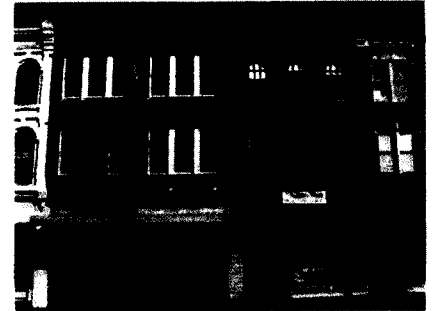
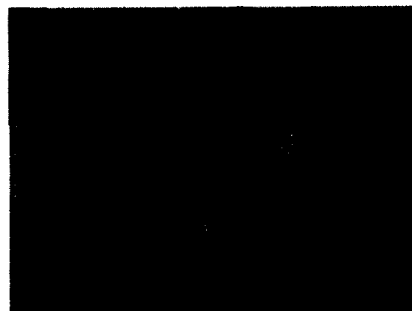
#### MASSING AND COMPOSITION

It is critically important that each building in Somerset Waters be designed with proper massing and proportion based on the historic architectural style chosen. Some general guidelines for building composition are as follows:

**Urban buildings shall** exhibit historically and architecturally appropriate massing and composition subject to the approval of the Design Review Committee (DRC). They should include a base, middle, and a cap, with transition lines between each. The specific locations of these transition lines are determined primarily by the overall height of the building and that of the adjacent buildings.

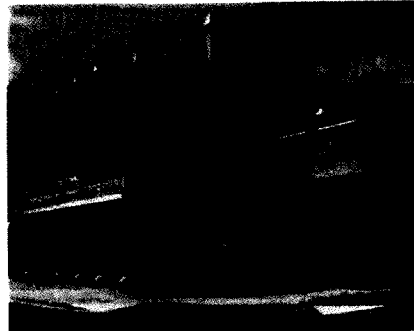
- \* The middle of the building should be differentiated from the base by a transition line that is generally placed at the top of the first floor.
- \* The cap of the building should be differentiated from the middle by a second transition line, usually below the top floor windows. In many cases this row of windows may be square or shorter than the floors below.
- \* The base of the building is encouraged to incorporate corbelling, molding, string coursing, ornamentation, or changes in material or color.
- \* The cap, or cornice of the building is frequently between one fifteenth and one eighteenth of the building height, depending on the total height and architectural style.

**All buildings are encouraged** to have offsets within the primary elevations facing the public right-of-way. This helps to break up large facades and create a more varied streetscape. These offsets may be either projections from or recesses in the primary facade.





## EXTERIOR ARCHITECTURAL DETAIL AND MATERIALS



**Materials** shall be selected and used with consideration to the building scale, color scheme, and building mass. Materials shall exhibit the characteristics of a permanent solid material that will stand the test of time.

All materials, finishes, and color schemes used in Somerset Waters shall require Design Review Committee approval. The following is a listing of details and materials generally permitted for roofs, exterior veneers, doors, windows, awnings, gutters and downspouts, screening and signage.



### A. ROOFS

#### Acceptable Roof Materials:

- \* Metal Roofing Systems, standing seam (sloped roof forms) or flat seam (flat roof forms).
- \* Dimensional shingles with distinct profile
- \* Flat roof membrane system with parapet wall on all sides to screen roof and roof-mounted equipment.
- \* Slate
- \* Copper (preferably left to age naturally)

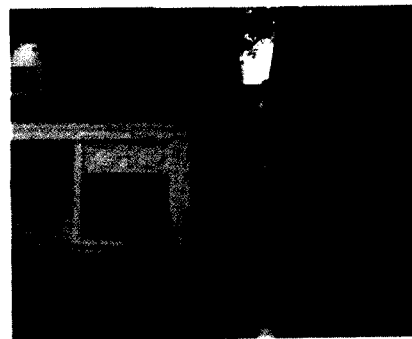
#### Roof Details

- \* Roof forms shall be consistent with the style and character of each building. Roofs shall be gable or hipped, or single low-slope with parapet wall. To emphasize the desired "village" character, residential roof forms shall be the primary roof type. Parapet roofs shall be used predominantly on single story structures to create height and street enclosure.
- \* All gable or hip roofs shall provide an appropriate eave for shadow lines.
- \* Roof vents and roof mounted mechanical equipment shall not be visible from public right-of-ways and shall be painted to match the roof color.
- \* Roof slopes shall be consistent with architectural style of buildings. Gable or hip roof slopes shall be 3:12 minimum. Slopes of 6:12 and above are encouraged. Porch roofs may be less, depending on the style of the building.
- \* Roof articulation, including chimneys, cupolas, monitors and dormers are encouraged to add additional detail to a sloping gable or hip roof profile. Chimneys may be stone or brick. Chimneys made of siding or synthetic stucco are prohibited where visible from the public right-of-way.

### B. EXTERIOR VENEERS

#### Acceptable Exterior Finishes

- \* Brick or painted brick
- \* Stone or cultured stone
- \* Real stucco applied to masonry with historically accurate details. (Stucco should be used in moderation and only where architecturally appropriate).
- \* Cementitious siding material
- \* Natural wood siding
- \* Smooth cut wood shingles or clapboard
- \* Board and batten



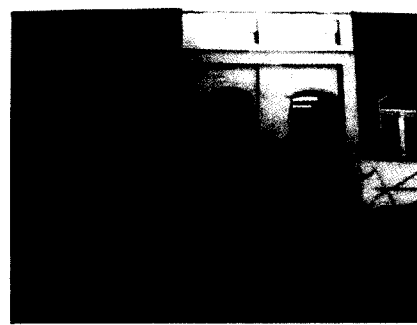
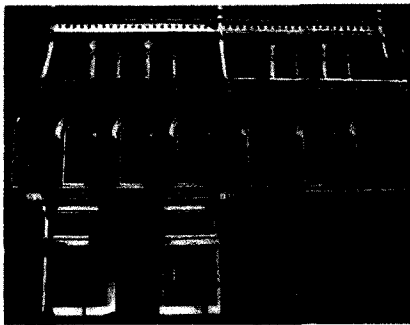


## DESIGN CODE

### URBAN CENTER ARCHITECTURAL CODE

#### *Exterior Colors*

- \* Colors within a façade shall be selected from a single quadrant of the color wheel.
- \* Exterior building material colors shall have a warm hue. Earth tones and other warm natural colors (including natural white tones for stucco walls) are acceptable.
- \* Trim and attached elements may be white or a darker or lighter saturation of the wall color, and therefore consistent and complimentary to the exterior building materials.
- \* Reflective and high sheen finishes shall not be used for exterior surfaces, with the exception of hardware items.



### C. DOORS AND WINDOWS

The design, location and style of doors and windows selected for a building are critical to the character, appearance, sense of mass, and functionality of the building. Doors and windows shall be designed to reinforce the building's architectural style.

#### *Window Materials:*

- \* Reflective mirrors or smoked glass is not permitted.
- \* Wood window frames are encouraged where appropriate.
- \* Vinyl or aluminum clad windows may be used.
- \* Clear aluminum storefront framing systems are unacceptable.

#### *Window Details:*

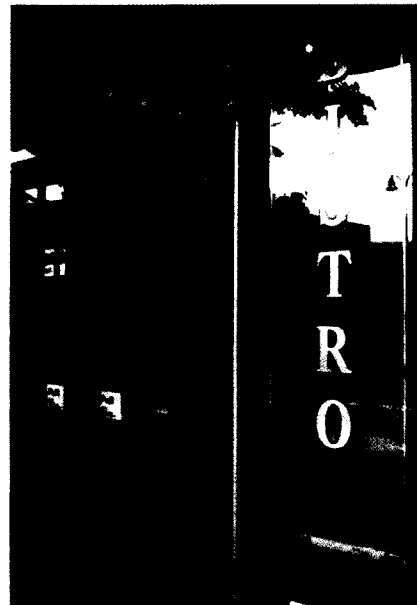
- \* Windows should be distinguished in the facade by the use of recessing, pediments, arches, etc.
- \* Windows shall be vertically proportioned casement or double hung (store fronts exempt).
- \* Storefront windows should be glazed with large panes of glass. Storefronts are generally at least 75% glass.
- \* Smaller individual windows may be glazed with small panes of glass.
- \* Windows shall generally be placed in symmetrical or balanced patterns.
- \* Providing operable shutters at double hung or casement windows for authenticity and detail are acceptable where applicable and consistent with the architectural style of the building. These shutters shall be one-half of the width of a window if done in pairs or full width if only one is provided. Shutters shall not be located on double or triple windows that are ganged together or on storefronts.

#### *Door Materials:*

- \* Solid wood doors are encouraged where appropriate.
- \* Clad doors may be used.

#### *Door Details:*

- \* Entryways should be recessed (at least 100 square feet)
- \* Main entry doors can range from single door (solid, half, three quarter, or full glass) to paired solid or French doors.
- \* Transom and sidelights can be appropriate when proportionate in scale to door opening. Muntin pattern shall be consistent with the door glass.
- \* Main entry door, if solid, shall be paneled. Panel design shall be two panel, four panel or six panel.
- \* Overhead fanlights above a door are acceptable if consistent with the door style and the architectural style of the building.



### D. AWNINGS

#### *Awning Materials:*

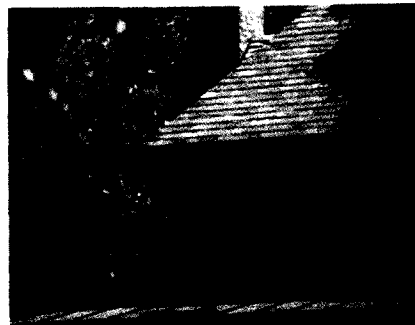
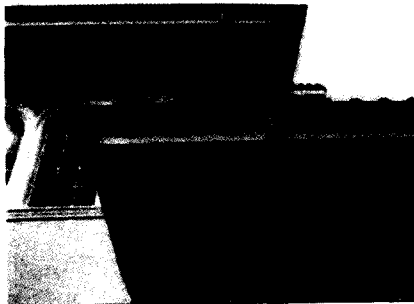
- \* High quality, heavy canvas awnings affixed to rust-resistant metal framing are generally acceptable.
- \* Painted metal awnings may be used when appropriate.





**Awning Details:**

- \* Style, shape, color and location of awnings shall be consistent with the desired character of the community.
- \* Awnings shall be a dark or muted color. Bright primary colors and multi-colored (more than 2) striped awnings are prohibited.
- \* It is appropriate to light the exterior face of awnings at night with historically styled gooseneck fixtures painted to be consistent with the establishment's color scheme and architectural style. Lighting provided below an awning shall be concealed if not decorative. Backlit translucent awnings are unacceptable.



**E. GUTTERS AND DOWNSPOUTS**

- \* Rain gutters and downspouts should be functional as well as provide desirable detail.
- \* Where practical, downspouts shall be located at the inside corners of buildings. Downspouts shall not be located on columns.
- \* Day lighting downspouts at the face of the building onto a public walk area are prohibited.
- \* Gutters shall be half-round profile and painted to match building facade or trim, or unpainted copper.

**F. ARCHITECTURAL SCREENING**

- \* All equipment, vents and other similar objects located on the building roof shall not be visible from public right-of-ways.
- \* Screening material shall be consistent with the materials and color scheme of the building.



**G. SIGNAGE**

*Note: The signage portion of the Architectural Code does not apply to townhouses. Exterior signs are not permitted on townhouse residences. Signage within the community shall follow the regulations set forth in the McKinney Signage Ordinance, Sec. 38-9 (Provisions For Signs in Downtown Commercial Historic District).*

All signage within Somerset Waters shall be designed to harmonize with the color scheme, proportions, and architectural character of the building to which it is mounted and the community as a whole. The following are general guidelines for the selection and use of signage within Somerset Waters. All signage is subject to the approval of the Design Review Committee.

**Acceptable Signage Types:**

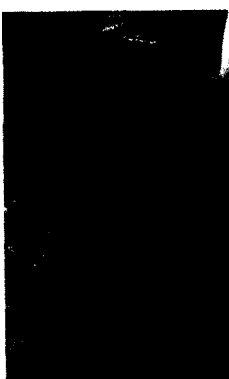
**Attached Signs:**

- \* Attached signs are those which are attached to, applied on, or supported by any part of the building, including the walls, windows, roof, or awning.
- \* Permitted types include, but are not limited to, sign bands, sign boards, hanging signs, projecting signs, name plate signs, window signs, awning signs, menu boards, vertical corner signs and painted murals.
- \* The cumulative area of all attached signs for a single establishment is limited to 1.5 times the linear footage of the individual storefront.
- \* The height and widths of the attached signs shall be in proportion to the area of the building where the sign will be located.

**Detached Signs:**

- \* Detached signs are those which are connected to the ground, and therefore not attached to any part of the building to which they are related.
- \* Permitted types include, but are not limited to, temporary directional (real estate) signs, sandwich boards, ground-mounted post signs, and unattached unique product signage.
- \* Permanent detached signage shall be located within 15 feet of the business to which it is related (if any).
- \* Pole-mounted banners/flags subject to DRC approval.

The images above and at right illustrate several of the acceptable attached and detached signage types to be implemented into Somerset Waters. The specific types pictured include awning signs, band signs, projecting blade signs, ground-mounted post signs, and window signs.





## DESIGN CODE

### URBAN CENTER ARCHITECTURAL CODE

#### Prohibited Signage Types:

- \* Prohibited attached sign types include: exterior internally lit signs, roof-mounted signs, aluminum box signs with vinyl face, and temporary "Sale" signs attached to the exterior of the building.
- \* Prohibited detached sign types include: ground-mounted pylon signs, oversized non-premises signage (billboards).
- \* Signs that are excessively lit, animated, wind-driven or flashing.
- \* Balloons hung outside of a business more than one business day.
- \* Signs made of plastic, paper, or fluorescent materials.

#### Criteria for Specific Acceptable Signage Types:

##### *Awning Signs (Images A and C):*

- \* An awning sign is one that is directly applied (generally painted) on the main body or fringe of an awning.
- \* Signs that are located on the main body of the awning shall be a maximum size of one-third of the awning area for a full-length storefront type awning, but can occupy the entire area of an entry-only awning or the end of an entrance canopy awning in which the flat area of the awning faces the adjacent street.
- \* Signage graphics on the fringe of the awning can occupy the total height of the awning fringe up to a maximum height of 9" tall.

##### *Band/Board Signs (Images B, C, D and E):*

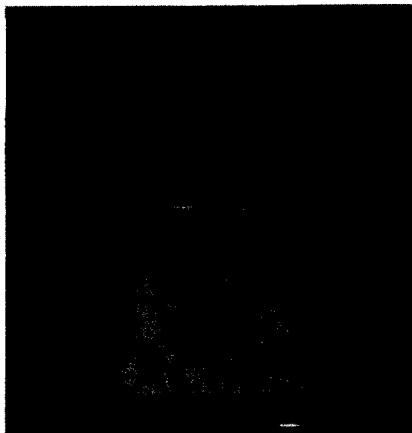
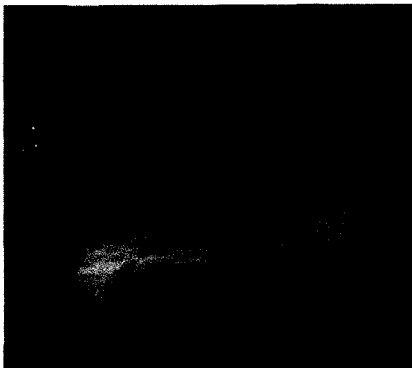
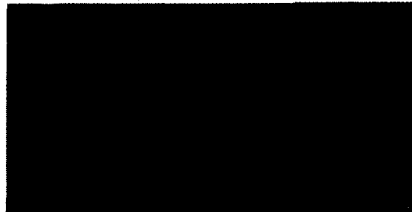
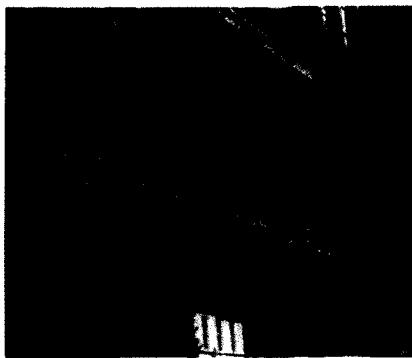
- \* Band signs consist of individual letters or continuous text in a thin sign band identifying the establishment. These signs are generally located directly above and centered on the entrance to the establishment.
- \* Board signs consist of a combination of graphics and/or text on a signboard of varying shape and size identifying the establishment. These signs may be located above or beside the entrance to the establishment.
- \* Both band and board signs are mounted to or attached to the building face and front-lit with a gooseneck type light fixture.
- \* Generally either a band or a board sign is permitted on a single establishment, not both.
- \* Sign material should be either wood or metal.

##### *Hanging/Blade Signs (Images F and G):*

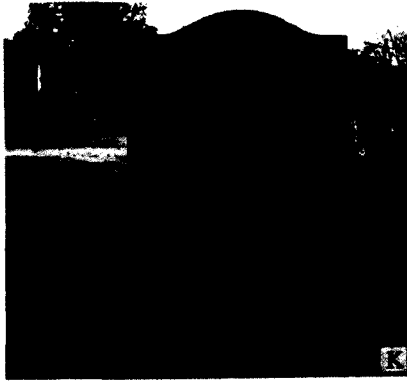
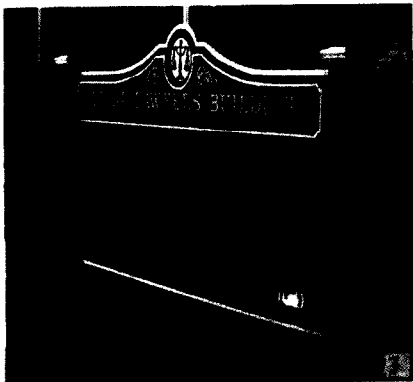
- \* Hanging signs are attached beneath the awning or canopy of a building. Hanging signs may be suspended parallel or perpendicular to the building face and may vary in size and shape.
- \* Blade signs (or projecting signs) are attached directly to the building or suspended from a bracket or bar. Blade signs are suspended perpendicular to the building face and may vary in size and shape.
- \* Hanging signs shall have at least 7' of clearance from the sidewalk. Blade signs shall have at least 8.5' of clearance from the sidewalk.
- \* Hanging signs may not extend beyond the awning or canopy projection. Blade signs may not project more than 5' from the building or more than 50% of the sidewalk width, whichever is less.
- \* Each face of a hanging sign shall not be more than 5 square feet. Each face of a blade sign shall not be more than 15 square feet.
- \* Sign material should be either wood or metal.

##### *Vertical Corner Signs (Images H and I):*

- \* Vertical corner signs are those that are vertically oriented and mounted to or near the corner of a building. They can only occur on the end of a building, at a corner street, or at a corner of an alley.
- \* Minimum height of the bottom of the sign shall be 12'-0" with a sign height maximum of 14'-0".
- \* Sign shall either project perpendicular from the side of the building or at a 45-degree angle to the corner of the building.
- \* Sign material should be either wood or metal and should be lit with gooseneck lights or surface mounted neon. No internally lit signs are acceptable.

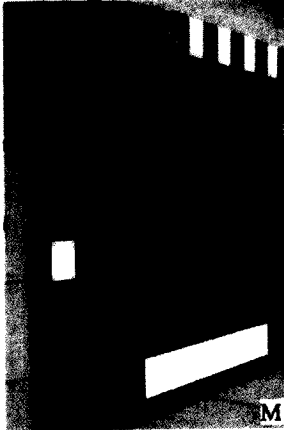
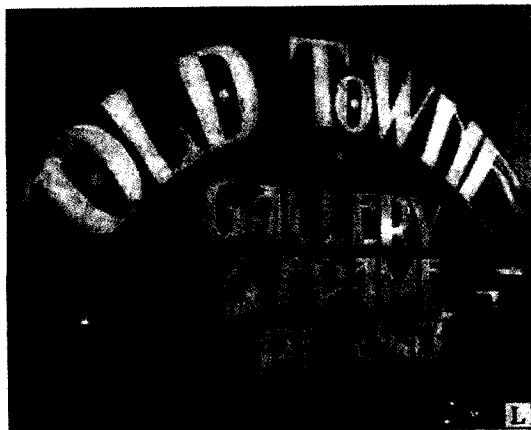






Ground Mounted Post Signs (Images J and K):

- \* Sign may be supported by one or two freestanding poles or posts.
- \* Posts shall be period-designed and architecturally appropriate.
- \* Signs may be double sided with each face not to exceed four square feet surface area.
- \* Signs shall not be back lit but may be lit with period goose neck style fixtures.
- \* Signs shall be within 15' of the façade of the related business and shall not obstruct driver or pedestrian views.

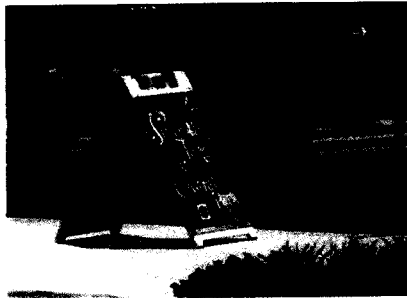
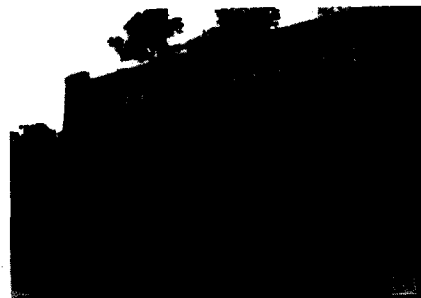


Window Signs (Images L and M):

- \* Window signs can either be of a neon design behind the glass storefront or paint/vinyl applied directly to inside pane of glass.
- \* Window signs applied directly to the glass shall cover no more than 40% of the total glass area (with the exception of neon)
- \* No signs or posters (temporary or permanent) can hang inside exterior windows closer than 12" from inside face of glass.
- \* Internally lit signs on the inside of an establishment are prohibited (with the exception of neon)

Portable Menu Boards and Sandwich Signs (Image O):

- \* Only one such sign shall be permitted per primary building entrance.
- \* Sign shall not occupy more than 6 square feet of sidewalk.
- \* Sandwich Signs shall have a maximum width of 2'-6" and maximum height of 4'-0". They shall be constructed of wood framed metal or signboard with wood frame. These signs should be brought inside the store at the close of business each day.



Painted Wall Art/Murals (Image N):

- \* Painted Wall Art includes images and murals painted directly on brick exterior walls of a business.
- \* Painted Wall Art shall not identify the specific business on which the art is being displayed or any of the actual products that could be for sale within that business.
- \* This type of sign does not apply towards maximum signage area requirements.

## FIRE PROTECTION REQUIREMENTS

Any and all construction within the Southern Land Community is subject to City of McKinney Fire Protection Requirements and the approval of the City of McKinney Fire Department.

### BUILDINGS:

- \* Townhomes that have three or more combined units shall be sprinklered.
- \* Commercial units of six thousand (6,000) square feet or more shall be sprinklered.
- \* Restaurants of five thousand (5,000) square feet or more shall be sprinklered.
- \* Buildings with the highest floor height exceeding thirty (30) feet shall require standpipes.
- \* Buildings requiring more than two hundred and fifty (250) feet as the hose lays from the fire lane to the lowest level of the fire department vehicle access shall require standpipes.
- \* Mixed-use occupancies (those with businesses on the lower floor and residential on the upper floor) shall be sprinklered according to NFPA 13.
- \* All civic and other commercial buildings shall be subject to commercial civil construction, building construction, fire sprinkler, fire alarm, kitchen hood, and fire service underground requirements.
- \* All residential buildings shall be subject to standard civil construction, building construction, and fire protection requirements.

### THOROUGHFARES:

- \* A twenty six (26) foot fire lane will be required where buildings are greater than thirty (30) feet from the lowest point of fire department access. (Ref. IFC 2003, Appendix D105)
- \* No overhead wires shall be permitted in T4 or T5 zones, or in areas where buildings exceed thirty (30) feet in height.
- \* Fire lanes that exceed one hundred and fifty (150) feet in length must have an approved turnaround.
- \* Dead end thoroughfares may not exceed six hundred (600) feet in length.
- \* All portions of all buildings shall be within one hundred and fifty (150) feet of a Fire Lane or public thoroughfare.
- \* A minimum of fourteen (14) feet clearance shall be maintained on all thoroughfares.
- \* Two (2) points of access shall be maintained at all times during the construction of the community, and fire lanes and fire hydrants shall be installed and operational prior to any vertical construction.





## APPENDIX

### TERMS AND DEFINITIONS

**Allee:** a regularly spaced and aligned row of trees usually planted along a Thoroughfare or Pedestrian Path.

**Ancillary Unit:** an apartment not greater than 600 square feet sharing ownership and utility connections with a Principal Building. An Ancillary Unit may or may not be within an outbuilding. Ancillary Units do not count toward maximum density calculations (see Tables 16 and 12).

**Apartment:** a dwelling unit sharing a building and a lot with other dwellings and/or uses. Apartments may be for rent or for sale as condominiums.

**Avenue (AV):** a thoroughfare of high vehicular capacity and low speed. Avenues are short distance connectors between urban centers. Avenues may be equipped with a landscaped median. Avenues become collectors upon exiting urban areas.

**Backbuilding:** a single-story structure connecting a principal building to an outbuilding (see Table 16).

**Bicycle Lane (BL):** a dedicated bicycle lane running within a moderate-speed vehicular thoroughfare, demarcated by striping.

**Bicycle Trail (BT):** a bicycle way running independently of a high-speed vehicular thoroughfare.

**Block:** the aggregate of private lots, passages, rear lanes and alleys, circumscribed by thoroughfares.

**Block Face:** the aggregate of all the building facades on one side of a block. The Block Face provides the context for establishing Architectural Harmony.

**Boulevard (BV):** a thoroughfare designed for high vehicular capacity and moderate speed. Boulevards are long-distance thoroughfares traversing urbanized areas. Boulevards are usually equipped with slip roads buffering sidewalks and buildings. Boulevards become arterials upon exiting urban areas.

**Building Disposition:** the placement of a building on its lot (see Table 9).

**Building Function:** the uses accommodated by a building and its lot. Functions are categorized as Restricted, Limited, or Open, according to the intensity of the use (see Tables 10 & 11).

**Building Height:** the vertical extent of a building measured in stories, not including a raised basement or a habitable attic. Height limits do not apply to masts, belfries, clock towers, chimney flues, water tanks, elevator bulkheads and similar structures. Building Height shall be measured from the average grade of the enfronting thoroughfare (see Table 8).

**Building Type:** a structure category determined by function, disposition on the lot, and configuration, including frontage and height.

**Civic:** the term defining not-for-profit organizations dedicated to arts, culture, education, recreation, government, transit, and municipal parking.

**Civic Building:** a building designed specifically for a civic function. Civic Buildings shall not be subject to the requirements of Section 5. The particulars of their design shall be determined by Exception.

**Civic Parking Reserve:** parking structure or lot within a quarter-mile of the site that it serves. Space may be leased or bought from this Reserve to satisfy parking requirements.

**Civic Space:** an open area dedicated for public use. Civic Space types are defined by the combination of certain physical constants including the relationship between their intended use, their size, their landscaping and their enfronting buildings. See Table 13.

**Commercial:** the term collectively defining workplace, office and retail functions.

**Context:** surroundings made up of the particular combination of elements that create specific habitat.

**Corridor:** a lineal geographic system incorporating transportation and/or greenway trajectories. A transportation corridor may be a lineal urban Transect Zone.

**Courtyard Building:** a building that occupies the boundaries of its lot while internally defining one or more private patios.

**Curb:** the edge of the vehicular pavement detailed as a raised curb or flush to a swale. The Curb usually incorporates the drainage system (see Table 4).

**Density:** the number of dwelling units within a standard measure of land area, usually given as units per acre (see Section 3.4).

**Design Speed:** is the velocity at which a thoroughfare tends to be driven without the constraints of signage or enforcement. There are three ranges of speed: Very Low: (below 20 MPH); Low: (20-25 MPH); Moderate: (25-35 MPH); High: (above 35 MPH). Lane width is determined by desired design speed.

**Drive (DR):** A vehicular and pedestrian thoroughfare type that occurs as a boundary between a natural condition and an urbanized area.

**Driveway:** a vehicular lane within a lot, usually leading to a garage. A Driveway in the First Layer may be used for parking if it is no more than 18 feet wide, thereby becoming subject to the constraints of a parking lot.

**Edgeward Building:** a building that occupies the center of its lot with setbacks on all sides.

**Elevation:** an exterior wall of a building not along a Frontage Line. See: **Facade** (Table 16)

**Enfront:** to place an element along a frontage line, as in "porches enfront the street."

**Entrance, Principal:** the main point of access of pedestrians into a building.

**Exception:** a variance that permits a practice that is not consistent with a provision or Intent of this Code. Exceptions are usually granted only by the Board of Appeals.

**Facade:** the exterior wall of a building that is set along a Frontage Line (see **Elevation; Frontage Line**).

**Frontage Line:** those lot lines that coincide with a public frontage. Facades along Frontage Lines define the public realm and are therefore more regulated than the elevations that coincide with other Lot Lines (see Table 16).

**Greenway:** an open space corridor in largely natural conditions which may include Trails for bicycles and pedestrians.

**Inside Turning Radius:** the curved edge of a thoroughfare at an intersection, measured at the inside edge of the vehicular tracking. The smaller the Turning Radius, the smaller the pedestrian crossing distance and the more slowly the vehicle is forced to make the turn. (See Tables 3 and 16)

**Layer:** a range of depth of a lot within which certain elements are permitted (see Table 16).

**Liner Building:** a building specifically designed to mask a parking lot or a parking garage from a frontage. A Liner Building, if less than 30 feet deep and two stories, shall be exempt from parking requirements.

**Live-Work:** a dwelling unit that contains, to a limited extent, a commercial component. A Live-Work Unit is a fee-simple unit on its own lot with the commercial component limited to the ground level. (Syn.: Flexhouse.) (See **Work-Live**.)

**Lodging:** premises available for daily and weekly renting of bedrooms. The area allocated for food service shall be calculated and provided with parking according to retail use.

**Lot Line:** the boundary that legally and geometrically demarcates a lot (see Frontage Line). Such lines appear graphically on Community and Site Plans. Codes reference lot lines as the baseline for measuring setbacks (see Tables 16 and 14G).

**Lot Width:** the length of the principal Frontage Line of a lot.

**Manufacturing:** premises available for the creation, assemblage and/or repair of artifacts, using table-mounted electrical machinery and including their retail sale.

**Neighborhood:** a mostly residential area, often with a recognizable edge. For the purposes of this SmartCode, a "complete neighborhood" is further defined as consisting of one pedestrian shed (1/2 mile diameter) with a mixed-use center.

**Office:** premises available for the transaction of general business but excluding retail, artisanal and manufacturing uses.

**Outbuilding:** an accessory building, usually located towards the rear of the same lot as a Principal Building. It is sometimes connected to the principal building by a Backbuilding. Outbuildings shall not exceed 600 square feet of habitable space, excluding parking areas (see Table 16).

**Parking Structure:** a building containing two or more stories of parking.





Parking Structures shall have Liner Buildings at the first story or higher.

**Parkway (PKWY):** A long-distance boulevard that connects urban centers.

**Passage (PS):** a pedestrian connector passing between buildings, providing shortcuts through long blocks and connecting rear parking areas to frontages. Passages may be roofed over.

**Path (PT):** a pedestrian way traversing a park or rural area, with landscape matching the contiguous open space. Paths should connect directly with the urban sidewalk network.

**Pedestrian Shed:** an area defined by the average distance that may be traversed at an easy walking pace from its edge to its center. This distance is applied to determine the size of a Neighborhood or extent of a Community. A standard Pedestrian Shed is one quarter of a mile radius or 1320 feet. With transit available or proposed, a Long Pedestrian Shed has an average walking distance of a half-mile or 2640 feet. Pedestrian Sheds are oriented toward a central destination containing one or more important intersections, meeting places, civic spaces, civic buildings, and the capacity to accommodate a T5 Transect Zone in the future. Sometimes called walkshed or walkable catchment.

**Planter:** the element of the public streetscape which accommodates street trees. Planters may be continuous or individual.

**Principal Building:** the main building on a lot, usually located toward the frontage (see Table 16).

**Private Frontage:** the privately held layer between the frontage line and the principal building facade. The structures and landscaping within the Private Frontage may be held to specific standards. The variables of Private Frontage are the depth of the setback and the combination of architectural elements such as fences, stoops, porches and galleries (see Table 7).

**Public Frontage:** the area between the curb of the vehicular lanes and the Frontage Line. Elements of the Public Frontage include the type of curb, walk, planter, street tree and streetlight (see Table 4).

**Rear Alley (AL):** a vehicular driveway located to the rear of lots providing access to service areas and parking, and containing utility easements. Alleys should be paved from building face to building face, with drainage by inverted crown at the center or with roll curbs at the edges.

**Rear Lane (LA):** a vehicular driveway located to the rear of lots providing access to parking and outbuildings and containing utility easements. Rear lanes may be paved lightly to driveway standards. Its streetscape consists of gravel or landscaped edges, no raised curb and is drained by percolation.

**Rearyard Building:** a building that occupies the full frontage line, leaving the rear of the lot as the sole yard. This is a more urban type, as the continuous facade spatially defines the public thoroughfare. For its residential function, this type yields a rowhouse. For its commercial function, the rear yard can accommodate substantial parking.

**Residential:** premises available for long-term human dwelling.

**Retail:** premises available for the sale of merchandise and food service.

**Retail Frontage Line:** Frontage Lines designated on a Community Plan that require the provision of a Shopfront, causing the ground level to be available for retail use.

**Road (RD):** a local, rural and suburban thoroughfare of low vehicular speed and capacity. Its public frontage consists of swales drained by percolation and a walking path or bicycle trail along one or both sides. The landscaping consists of multiple species composed in naturalistic clusters. This type is allocated to the more rural Transect Zones (T1-T3).

**Rural Boundary Line:** the extent of potential urban growth as determined by existing geographical determinants. The rural boundary is permanent.

**Setback:** the area of a lot measured from the lot line to a building facade or elevation. This area must be maintained clear of permanent structures with the exception of: galleries, fences, garden walls, arcades, porches, stoops, balconies, bay windows, terraces and decks (that align with the first story level) which are permitted to encroach into the Setback. (See Section 5.2.1 and Table 14G)

**Sidyard Building:** a building that occupies one side of the lot with a setback to the other side.

**Sidewalk:** the paved layer of the public frontage dedicated exclusively to pedestrian activity.

**Slip Road (SR):** A one-way, low-capacity, low speed alternative route that

runs parallel to a high-capacity, high-speed thoroughfare such as a parkway.

**Specialized Building:** a building that is not subject to Residential, Commercial, or Lodging classification. Most specialized buildings are dedicated to manufacturing and transportation, and are distorted by the trajectories of machinery.

**Specialized District (SD):** Specialized District designations shall be assigned to areas that, by their intrinsic function, disposition, or configuration, cannot conform to one of the six normative Transect Zones or four Community Types specified by this Code. Typical Districts may include large parks, institutional campuses, refinery sites, airports, etc.

**Story:** a habitable level within a building of no more than 14 feet in height from finished floor to finished ceiling. Attics and raised basements are not considered stories for the purposes of determining building height.

**Streamside Corridor:** the zone within which a waterway flows, its width to be variably interpreted according to the Transect Zone.

**Street (ST):** a local urban thoroughfare of low speed and capacity. Its public frontage consists of raised curbs drained by inlets and sidewalks separated from the vehicular lanes by a planter and parking on both sides. The landscaping consists of regularly placed street trees. This type is permitted within the more urban Transect Zones (T4-T6).

**Streetscape:** the urban element that establishes the major part of the public realm. The streetscape is composed of thoroughfares (travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians) as well as the visible private frontages (building facades and elevations, porches, yards, fences, awnings, etc.), and the amenities of the public frontages (street trees and plantings, benches, streetlights, etc.).

**Terminated Vista:** a location at the axial conclusion of a thoroughfare. A building located at a Terminated Vista designated on a Community Plan is required to be designed in response to the axis.

**Thoroughfare:** a vehicular way incorporating moving lanes and parking lanes within a right-of-way (see Tables 3 and 16).

**TND:** Traditional Neighborhood Development. A Community Type consisting of one or more pedestrian sheds plus a mixed-use center or corridor. (Syn.: Village, Urban Village). TND is permitted by Right in the Controlled and Intended Growth Sectors.

**Town Center:** the mixed-use center or main commercial corridor of a community. A town center in a hamlet or small TND may consist of little more than a meeting hall, corner store, and main civic space. A town center for RCD or TOD communities may be a substantial downtown commercial area, often connected to other town centers by transit.

**Transect:** a system of ordering human habitats in a range from the most natural to the most urban. The SmartCode is based upon six Transect Zones which describe the physical character of place at any scale, according to the density and intensity of land use and urbanism.

**Transect Zone (T-Zone):** Transect Zones are administratively similar to the land-use zones in conventional codes, except that in addition to the usual building use, density, height, and setback requirements, other elements of the intended habitat are integrated, including those of the private lot and building and the enfronting public streetscape. The elements are determined by their location on the Transect scale. The T-Zones are: T1 Natural, T2 Rural, T3 Sub-Urban, T4 General Urban, T5 Urban Center, and T6 Urban Core. (See Table 1)

**Type:** a form category determined by function, disposition, and configuration, including size or extent. There are community types, street types, civic space types, etc. See also: **Building Type**.

**Variance:** an administrative technique granting relief from the provisions of a code. There are two types of variances: Warrants and Exceptions (see Section 1.5).

*CREDIT: Terms and definitions courtesy of Smart Code V 7.0; Duany Plater-Zyberk & Company. All references are to Tables in the Smart Code document, available for public use at [www.dpz.com](http://www.dpz.com).*